



May 26, 2023

TO: Planning Commission

FROM: Comprehensive and Neighborhood Planning Committee

SUBJECT: Proposed Amendments to the *Ford Site Zoning and Public Realm Master Plan* related to the Proposed University of Saint Thomas Athletic Complex (Zoning File No. 23-011-392 & 23-011-403)

Background

The *Ford Site Zoning and Public Realm Master Plan* (Master Plan) was adopted by the City Council in September 2017. In June 2018, Ford Land announced Ryan Companies (Ryan) as the Master Developer for the site and Ryan purchased the site soon thereafter. Construction of infrastructure and vertical projects began in 2019 and is anticipated to continue for the next decade. As anticipated, there have been several amendments to the Master Plan as development has happened and market and site realities have evolved. The current version of the Master Plan can be found [here](#).

Project Overview

The present package of amendments, submitted to the City of Saint Paul (City), relates to a proposed athletic complex for the University of Saint Thomas (UST) that would be constructed on 13.66 acres of land historically known as the Canadian Pacific (CP) rail yard parcels in the southeast corner of the Master Plan area (Figure 1). The site is bounded by parkland to the west, grade change and single-family residential to the south, grade change and a rail spur to the east, and the Highland Ballfields and undeveloped Ryan-owned parcel to the north. The proposed development includes baseball and softball facilities, an indoor practice facility, and a surface parking lot (approximately 330 spaces) (Figure 2). Construction timing is dependent on private financing.

Development of the CP parcels was not part of the original Ryan development plan and the proposed UST athletic complex was not anticipated when the Master Plan was adopted in 2017; this requires changes to the Master Plan and Zoning Code. This zoning application to amend the Master Plan and Zoning Code reflects an updated version of the application submitted and preliminarily processed but not approved in the spring and summer of 2022. The initial application was withdrawn in August 2022 to redesign the site plan without a proposed hockey facility, which has affected the requested Master Plan amendments. Additional information on the differences between applications can be found below. This report analyzes the proposed amendments and provides staff recommendations for Planning Commission consideration.

Figure 1: Project Boundaries

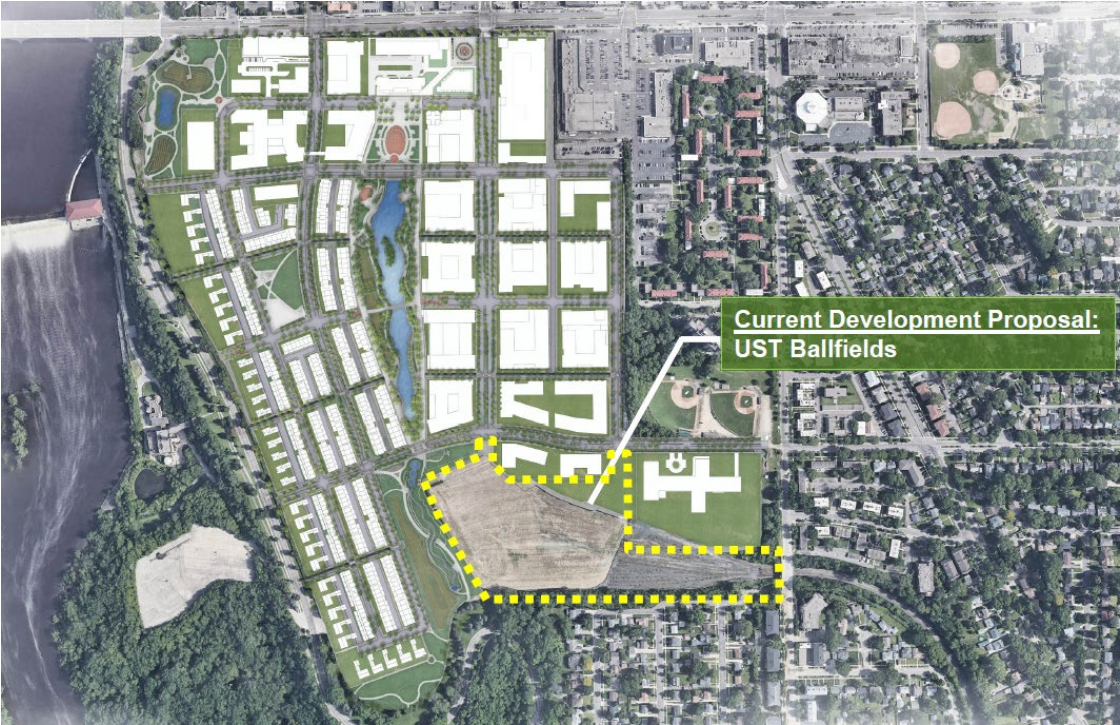
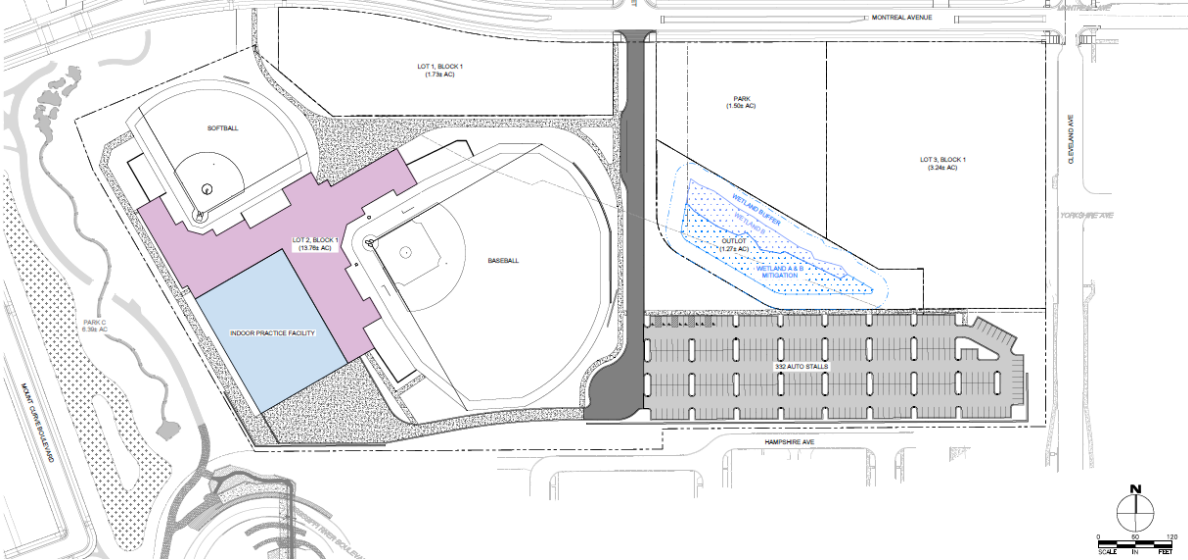


Figure 2: Draft Site Plan of UST Athletic Complex Provided by Ryan Companies (illustrative and subject to change)



Vision and Guiding Principles

The Master Plan establishes a vision and guiding principles for the development of the Highland Bridge site. The vision statement is “[t]he redeveloped Ford [Highland Bridge] site will balance economic, social and environmental sustainability in a way that conserves and improves the qualities and characteristics of the unique Highland Park neighborhood and Mississippi River valley in which it sits while advancing the City’s economic wealth and community goals, resulting in a forward-thinking 21st Century development.” Guiding principles (p. 18-19) provide additional direction regarding the mix of uses and activities, housing variety, jobs and tax base, energy and sustainability, transportation choice, and parks and amenities.

While this specific proposal was not anticipated when the Master Plan was written, a similar recreational use of the southeast portion of the Highland Bridge site was envisioned during Master Plan development due to interest from several private groups. Per a memo from the CNPC to the Planning Commission dated July 21, 2017, “[t]he City strongly supports the inclusion of recreational field space on the redeveloped Ford site. After testing various locations and configurations for this type of space on the site relative to other uses and their location, the southeast corner was ultimately identified as the best location for recreational fields.” The *Recreational Fields* section of the Master Plan (p. 139) states, “fields would be best located on the southeast corner of the [Highland Bridge] site to minimize any negative impacts of activity and lights on residential properties” and that they “could be connected to the wider public park network through bike and pedestrian trails.” Figure 3, below, shows the illustrative plan developed by the City in 2017 to depict the potential development that could occur under the proposed (and eventually adopted) zoning.

Figure 3: City Illustrative Plan, 2017



Relationship to Adjacent Properties

Block 33. The proposed UST project does not include any change to the overall quantity of affordable housing units per the Master Plan and Redevelopment Agreement requirement for 20 percent of total housing units to be affordable. The proposed project will not affect the affordable housing area median income targets nor the mix of affordable housing types. Ryan intends to maintain the 110 units of affordable housing originally planned for Block 33 (north of project site, west of Finn Street). Due to this proposed project, Ryan is applying to make a minor adjustment to the Block 33 parcel line via a plat by modifying the small, unusable triangle at the southern end of the parcel. See Figure 4 for a conceptual program layout.

Block 34. The proposed UST project does not include any change to the land use or size of development originally anticipated on Block 34 (north of project site, east of Finn Street). Ryan would like to reduce the parcel area to accommodate diverted and dedicated parkland and the wetland area required by the proposed project, while maintaining the area to accommodate 100,000 square feet of office space as originally anticipated in the Master Plan. Due to changes in market conditions since the 2019 Ryan Development plan, an office building in this portion of the development may be unlikely; however, the size and shape of Block 34 will allow for a variety of potential uses to activate this corner and meet the minimum assessment requirement per the Redevelopment Agreement with the City. The project team is exploring ways to utilize the proposed surface parking lot associated with the UST project as a shared parking area with Block 34 and other nearby developments. See Figure 4 for a conceptual program layout.

Figure 4: Conceptual Program Layout / Rendered Site Plan



Other Processes

There are several other processes and approvals required for the UST project that will be processed concurrently with this application, including:

AUAR Update

A process to update the Alternative Urban Areawide Review (AUAR), the environmental review document and mitigation plan approved by the City in 2019, restarted on January 31, 2023. The City opted to expand the scope and complete the update for the entire Highland Bridge site concurrently with the proposed UST project, given that an update would be required in the short-term regardless (AUAR updates are required every five years). The update will account for development to-date and the UST proposal in its analysis of a variety of environmental topics. Included in this analysis is a traffic study that will identify changes from the 2019 estimate and any alteration of mitigation strategies. Any Master Plan amendments relating to the UST project cannot be approved or adopted prior to the completion of the AUAR update.

Parkland Dedication and Diversion

Due to site planning needs and the proposed development of the CP parcels, the City's Department of Parks and Recreation, along with the Parks and Recreation Commission, will be reviewing a separate proposal to divert parkland area and meet parkland dedication requirements.

Legal and Financial Agreements

In 2019, the City and Ryan executed a Redevelopment Agreement that addressed issues such as tax increment financing (TIF) and affordable housing requirements. Amendments may be necessary to ensure that the obligations and expectations set forth in 2019 will be met in the future.

Other Reviews

In addition to the above, the Ryan and UST project team is applying to the City for a subdivision plat, wetland review, and Site Plan review, which will require approval by all City departments. The project team is also considering an application for a variance to the Metropolitan Airports Commission (MAC).


Master Plan Changes Proposed by Ryan

Ryan is proposing six amendments to the Master Plan. This section describes the proposed amendments with information on current Master Plan requirements, proposed changes, differences as compared to the 2022 proposal, and staff recommendations.

Exclusion to the Floor Area Ratio (FAR) Requirements Within the F6 District for Civic & Institutional Uses.

Current Master Plan	Ryan Proposal	Status
The Master Plan and Zoning Code require a FAR range of 1.0 (minimum) to 3.0 (maximum) for any development in the F6 zoning district.	Exclude civic & institutional buildings in the F6 zoning district from the minimum FAR requirement.	Similar to 2022 proposal with updated staff recommendation.

Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
1	31, 67	66.931

Discussion	
<p>FAR is a development regulation that establishes minimum requirements and maximum thresholds for building mass. FAR is defined as “[t]he total floor area of all buildings or structures on a zoning lot divided by the area of said lot” (Zoning Code Sec. 60.207). A minimum FAR ensures that a base level of development intensity is provided on a site and a maximum FAR limits the size of buildings in proportion to the site. For example, a FAR of 1.0 means that the gross square footage of the building may be equal to the gross square footage of the lot area (i.e., a 1:1 ratio).</p> <p>Civic & institutional uses are often characterized by large open spaces between and around buildings in a campus-like setting, typically leading to low FARs. The low FAR usually found with civic & institutional uses is consistent with the description of the F6 Gateway zoning district (see Figure 5) as “attractive gateways to the site” and compatible with existing open spaces (Highland Ball, Gateway Park) and future parkland in the F6 district areas. The UST project proposed within the F6 district includes ballfields, plaza and outdoor gathering spaces, areas for circulation through the site, and parking to accommodate the parking demand of spectators and users of nearby spaces, all of which contribute to project vibrancy but do not allow the project to meet the minimum FAR requirement of 1.0. The proposal to exclude civic & institutional uses from the minimum FAR requirement is consistent with the F6 district, surrounding uses, and the typical development of civic & institutional buildings. Because of this, it is appropriate to exclude civic & institutional uses from the minimum FAR requirement in the F6 zoning district.</p>	<p>Figure 5: F6 Zoning Districts</p> 

As build out of the Highland Bridge site continues, it will likely be desirable to consider building small structures on parkland to serve the needs of park users (e.g., shelters, restrooms, storage, etc.). These structures are considered civic & institutional uses. It would be beneficial to exclude civic & institutional uses from the minimum FAR requirements in all F districts, not only in the F6 district; without a sitewide exemption, those projects will require a variance.

It is important to address any unintended design consequences of exempting civic & institutional buildings from the minimum FAR requirement. As such, minor updates to Master Plan Section 5.2, Site-Wide Design Standards, may also be required.

Recommendation

Exempt civic & institutional uses/buildings from the minimum FAR requirement in all F districts. Update design standards G17 and G19 for clarity.

Modification to Table 4.7 for Civic & Institutional Uses as a Land Use Category and Setting the Maximum Spaces Allowed as Provided in Section 63.207 of the Zoning Code.

Current Master Plan	Ryan Proposal	Status
The Master Plan and Zoning Code provide maximum parking thresholds for all non-residential uses (1 space per 200 square feet GFA).	Include civic & institutional uses as a separate land use category with maximum parking thresholds per Sec. 63.207 of the Zoning Code.	New.

Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
2	58	66.942

Discussion

Maximum parking thresholds ensure that vehicular parking space does not negatively affect the built environment. The Master Plan provides maximum parking requirements by land use category for non-residential, residential, and residential congregate living based on gross floor area (GFA). Civic & institutional uses vary immensely (e.g., day care, college, public and private parks/playgrounds, recreation, religious institutions, etc.) and commonly include spaces for public gathering, which do not contribute to the calculation of GFA. Subjecting all civic & institutional uses to the same parking maximum as all non-residential uses does not adequately regulate nor acknowledge the variations possible within the land use category. The Zoning Code establishes parking maximums for a range of land uses within the civic & institutional land use category and is better suited to regulate maximum parking for this land use category in the Highland Bridge site.

The UST project includes large, activated spaces within and outside of buildings. Maximum allowed parking spaces for the outdoor ballfield stadiums would more appropriately be calculated by seat, anticipating spectator attendance, rather than by the GFA of the indoor concourse. For example, the Zoning Code Sec. 63.207 establishes a parking requirement for this use as 2 spaces per 4 seats or 8 feet of benches (note: bicycle parking requirements

are calculated by GFA (1 space per 5,000 square feet GFA)). The proposed amendment allows for the maximum number of parking spaces to better align with anticipated stadium attendance associated with the UST development and the future redevelopment of the Highland Ballfields.

Recommendation
Add civic & institutional uses as a separate land use category with maximum parking thresholds per Sec. 63.207 of the Zoning Code.

Exclusion to the Limitation of Surface Parking Spaces Within the Southeasterly F6 District for Civic & Institutional Uses.

Current Master Plan	Ryan Proposal	Status
The Master Plan establishes a maximum of 20 surface parking spaces per development block (project).	Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasterly F6 district.	New.

Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
3	58	N/A

Discussion
<p>The Master Plan establishes a maximum amount of surface parking that can be provided on a project-by-project basis to eliminate the opportunity for large surface parking lots to dominate blocks in Highland Bridge. This well-intended maximum threshold contributes to the walkable, urban neighborhood that the Master Plan intends to provide. The F6 district envisions a range of employment, office, and civic & institutional uses to serve as gateways into Highland Bridge. Civic & institutional uses, such as recreational facilities, are often part of an overall campus design where there are broad goals for building placement and site layout, rather than an urban neighborhood and street grid layout as envisioned by the Master Plan. These variable uses can provide vibrant, activated spaces in Highland Bridge despite being a development type with a larger footprint and related street network.</p> <p>This proposed amendment would allow for the development of a surface parking lot up to the off-street parking maximum per Sec. 63.207 associated with the proposed UST project. The proposed UST project is intended to be phased based on fundraising efforts and site usage and demand. The proposed surface parking lot, located on the southeastern portion of the site, is envisioned to be a temporary development scenario that redevelops into a parking structure when the proper demand and funding become available. In the interim, the surface parking lot is expected to be shared with adjacent areas such as the parkland to the north and the future Block 34 development. The surface parking lot provides flexibility for the buildout of the Shared Transportation Corridor (see below) through the site, connecting the CP Spur to Highland Bridge. The proposed amendment would also provide flexibility for the future redevelopment of the Highland Ballfields, which are located northeast of the UST project.</p>

Recommendation
Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasterly F6 district.

Exclusion to the Maximum Public Right-of-Way Setback Within the F6 District for Civic & Institutional Uses.

Current Master Plan	Ryan Proposal	Status
The Master Plan and Zoning Code establish a maximum building setback of 15' from the ROW in the F6 district with 60% of the building façade required within the maximum setback.	Exempt civic & institutional buildings from the maximum ROW setback in the F6 district.	Similar to 2022 proposal.

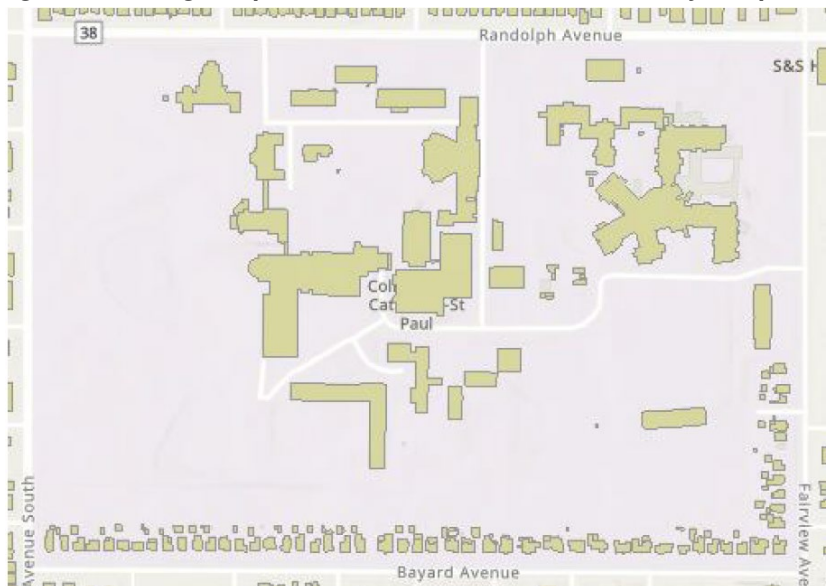
Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
4	93	66.931

Discussion

The F6 district envisions a range of uses and building types that can serve as gateways into the Highland Bridge site. The variety of building types found with civic & institutional uses can contribute to the walkable, urban neighborhood without placing buildings close to the street in the same way mixed-use and multifamily buildings do in other F districts. Therefore, the requirement to place all buildings close to the street limits the building patterns possible with civic & institutional development and does not consider project scenarios such as recreational facilities, which require specific siting for sun angles and large plaza spaces for queuing of spectators. In other Saint Paul areas, institutional uses are commonly located in residential districts that have no maximum setback requirement (e.g., University of Saint Thomas (R2), Saint Catherine University (R3), Concordia Saint Paul (RT1)) (see Figure 6).

The requested exemption would allow the proposed UST buildings and ballfields to be sited based on the unique requirements of this development program. The requested exemption would also be appropriate for other civic & institutional uses in all F districts, not only in the F6 district. Similar to the FAR requirement, future small structures on public or private parkland would require a variance from this standard without the approval of the proposed exemption.

Figure 6: Building Footprints within Saint Catherine University Campus




Recommendation

Exempt civic & institutional uses/buildings from the maximum ROW setback and building façade placement requirement in all F districts.

Adjustment to the Information Within the Shared Transportation Corridor Description.

Current Master Plan	Ryan Proposal	Status
The Master Plan describes and illustrates a single Shared Transportation Corridor in a specific location that contains pedestrian and bicycle facilities with space for future transit, if feasible.	Revise the details of the Shared Transportation Corridor to allow flexibility for routing and engineering decisions as design progresses.	Similar to 2022 proposal with updated staff recommendation.

Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
5	125	N/A

Discussion	
<p>The Shared Transportation Corridor in the Master Plan recognizes the potential for a multimodal connection between the Highland Bridge site and CP Spur. The CP Spur is a five-mile rail corridor that once served the Ford site and enters at the southeast corner of the proposed UST development under a Cleveland Avenue bridge. Development of multimodal connections is somewhat uncertain, but the Master Plan establishes the requirement for a multimodal connection in the future. Pedestrian and bicycle infrastructure through the site is expected, while the feasibility of transit service is less likely in the short-term. The Blue Line/Riverview Connection Study is underway to determine feasibility and alternatives for connecting the Riverview Corridor to the Blue Line west of the Highland Bridge site (see Figure 7).</p> <p>The existing Master Plan narrative indicates uncertainty (e.g., “Planning and final decisions about how to reposition the railway will be determined in the coming years”), but also illustrates a conceptual Shared Transportation Corridor alignment on an exhibit. Alignment of public easement(s) for the Shared Transportation Corridor will be completed during site plan review and should not be depicted in the Master Plan. The Master Plan must maintain the ability for pedestrian, bicycle, and transit connections through the site with a narrative that supports reasonable engineering and design flexibility while also ensuring a usable multimodal connection will be established.</p>	<p>Figure 7: Blue Line / Riverview Connection Study Area</p> 

Recommendation
Generalize the Shared Transportation Corridor narrative to highlight connection locations and modes. Clarify that development in this area must preserve pedestrian and bicycle connections from the CP Spur to Highland Bridge through the development site with opportunities for future transit service. Clarify that the location of public easement(s) must be agreed upon prior to final site plan approval. Remove the dimension table and unnecessary components of exhibits.

Adjustments to the Open Space System for the Proposed Relocation of Community Green Space and Addition of CP Rail Parkland Dedication.

Current Master Plan	Ryan Proposal	Status
The Master Plan describes locations of open space. Related to this proposal, there is a 1.5-acre park located south of the Montreal and Finn intersection labeled "Community Green Space," since named "Míča Park," intended to "provide common areas to support future community gardens and other active and passive uses."	Relocate Míča Park through parkland diversion slightly to the east of its current location. Dedicate additional parkland to meet requirements of Sec. 69.511 due to the development of the CP parcels.	Similar to 2022 proposal.

Change # in Ryan List	Affected MP Page Number(s)	Zoning Code Section(s)
6	131, 132, 138	N/A

Discussion
<p>The open space system consists of a combination of parkland, privately owned public space (POPS), and stormwater infrastructure. Per City and State regulations, a reasonable portion of buildable land must be dedicated for parks at the time of development. Dedication took place for the redevelopment area in 2019, but with the addition of the CP parcels to the development, additional parkland dedication is required per Sec. 69.511. In addition to new dedicated parkland, the applicant is proposing to shift and reconfigure the existing concept for Míča Park to accommodate the proposed UST project while continuing to meet the original Highland Bridge parkland dedication.</p> <p>The existing Master Plan did not assume development on the CP parcels and depicts Míča Park in a jagged shape due, in part, to the jagged CP parcel property line south of the Montreal and Finn intersection. The proposed parks and open space changes may positively affect the Master Plan's park system because Míča Park is expected to have a more usable parcel shape and even grade.</p> <p>There is a separate parkland diversion (due to shifting parkland) and dedication review underway with the Department of Parks and Recreation that will run concurrently with this Master Plan and Zoning Code amendment process. Final decisions regarding the location and design of parks will be subject to approval by the Department of Parks and Recreation. Final decisions on parkland diversion and dedication are made by City Council.</p>

Recommendation
Relocate Míča Park to meet the needs of the proposed UST development and add open space to meet parkland dedication requirements and improve the usable parkland area, depicted in the Open Space System map. Update the Open Space System map and add language to clarify that final location and design are subject to approval by the City. Update the summary of Community Green Space (Míča Park) in the Master Plan. Defer to the Department of Parks and Recreation for final location and design.

Comprehensive and Neighborhood Planning Committee

Comments and Response

On March 1, 2023, the Comprehensive and Neighborhood Planning Committee (CNPC) provided feedback on the proposed Master Plan amendments. Key comments and staff responses are summarized below.

- **Surface Parking Tradeoffs.** The existing Master Plan limits surface parking to 20 spaces per project. Projects may provide more than 20 spaces if the parking is structured, up to the maximum allowed by the Master Plan or Zoning Code. If civic & institutional uses are exempted from the 20 surface parking space maximum in the southeasterly F6 district, as explained above, the proposed UST surface parking lot (approximately 330 spaces) will be allowed. The CNPC expressed concern for the size of the proposed surface parking lot. Aside from the Master Plan limits, the proposed surface parking lot is within the maximum parking allowed for this land use per Zoning Code Section 63.207. While surface parking is not ideal in Highland Bridge, the following tradeoffs contribute to staff's recommendation in support of the Master Plan amendment:
 - **Public Access.** The surface parking lot is expected to be publicly accessible by users of adjacent areas, such as the parks to the north (Míča Park) and west (Ujčí Makhá Park), Highland Ballfields, and future Block 34 development, whenever UST events are not occurring (see below). The shared use of the parking area between UST and Highland Ball could greatly benefit the future redevelopment of the Highland Ballfields by decreasing the amount of land needed for an independent parking lot to serve the Highland Ballfields. UST is having ongoing conversations with adjacent stakeholders to determine the most effective way to share use of the surface parking lot.
 - **Shared Transportation Corridor Flexibility.** The development of the CP parcels by UST allows the opportunity for future development of the Shared Transportation Corridor, connecting the CP Spur to Highland Bridge, consistent with the City's updated draft Bicycle Plan. Construction of a surface parking lot, rather than a parking structure, provides more flexibility for the future Shared Transportation Corridor alignment. A parking structure could block future transportation paths, while a surface parking lot can be more easily redeveloped at the appropriate time. The City's updated draft Bicycle Plan illustrates a conceptual alignment through the site. A surface parking lot provides more flexibility for the Shared Transportation Corridor to be consistent with the Bicycle Plan.
 - **Phased Development Scenario.** The proposed UST project is intended to be phased based on fundraising efforts and site usage and demand. The proposed surface parking lot is envisioned to be a temporary development scenario that potentially redevelops into a parking structure and Shared Transportation Corridor when the proper demand and funding become available, and the Shared Transportation Corridor is better defined.
 - **Limited in Scope.** The proposed Master Plan amendment (i.e., the exemption from the 20 surface parking space maximum) would apply only to civic & institutional uses in the F6 district in this portion of the Highland Bridge site. This restricts the exemption to only the

proposed UST project and the redevelopment of the Highland Ballfields. No other project in Highland Bridge would be exempt from the surface parking maximum unless a variance is approved.

- **Interim Use Permits.** The CNPC expressed interest in better understanding interim use permits related to the proposed UST surface parking lot. An interim use permit may be an option for imposing a timeframe in which the proposed surface parking lot may exist. Minn. Stat. § 462.3597, Subd. 1 defines an interim use as “a temporary use of property until a particular date, until the occurrence of a particular event, or until zoning regulations no longer permit it.” Minn. Stat. § 462.3597, Subd. 2 provides city councils with the legal authority to grant interim use permits. Minn. Stat. § 462.3597, Subd. 2, reads: “Zoning regulations may permit the governing body to allow interim uses. The regulations may set conditions on interim uses. The governing body may grant permission for an interim use of property if:
 - The use conforms to the zoning regulations;
 - The date or event that will terminate the use can be identified with certainty;
 - Permission of the use will not impose additional costs on the public if it is necessary for the public to take the property in the future; and
 - The user agrees to any conditions that the governing body deems appropriate for permission of the use.”

Because the proposed surface parking lot does not currently conform to the applicable regulations, a Master Plan amendment is required regardless of the interim use permit, but the Planning Commission may consider recommending that the applicant apply for an interim use permit to City Council concurrent with or after the adoption of the proposed Master Plan amendment.

- **Minimum Floor Area Ratio Exemption.** The existing Master Plan subjects all civic & institutional uses to the minimum floor area ratio required in the applicable F district; however, civic & institutional uses tend to have unique site designs with larger building footprints and open areas that differ from small lot, walkable, urban neighborhood form (see above). Exempting all civic & institutional uses from the minimum FAR requirement in all F districts is the most streamlined approach for administration of the Master Plan. The exemption will benefit future approvals of other uses within the land use category, such as small structures on public or private parkland. Without the approval of the proposed amendment, these uses would require a variance from the FAR requirement. Because of the limited anticipated build out of civic & institutional uses in Highland Bridge, and the low number of uses in the Zoning Code that are civic & institutional (see Figure 8), staff does not expect an exemption for the land use category to have negative effects on the built environment.

Figure 8: Example of Civic and Institutional Uses in the Zoning Code

Civic and Institutional Uses	
Club, fraternal organization, lodge hall	
College, university, seminary, etc.	Public and private park, playground
Community center	Religious institution
Day care	School, primary & secondary
Homeless services facility	Trade school, arts school, dance school, etc.
Museum	
Public library	

- Climate Action and Solar Energy.** The Saint Paul Climate Action & Resilience Plan identifies a goal of engaging major private institutions to be climate leaders by improving energy efficiency and reducing emissions to help the city become carbon neutral by 2050. The City also has sustainable development policies for public and privately developed projects receiving more than \$200,000 in public financing, and the entire Highland Bridge development is subject to this based on the significant public infrastructure investments made during initial build out. The Sustainable Building Ordinance is intended to help reduce environmental degradation and improve healthy living, and it requires new projects to comply with one of the eligible green building standards (e.g., LEED Certified Silver, Gold, or Platinum; B3; Green Communities; Green Star).

The Master Plan includes solar energy requirements to promote the safe, effective, and efficient use of solar energy systems that reduce on-site consumption of fossil fuels or utility-supplied electric energy. Active solar energy systems are allowed as an accessory use in all zoning districts where structures are allowed, and solar access must be preserved during building development. Solar energy systems are subject to height, setback, coverage, visibility, and aesthetic standards based on construction type (roof-mounted, ground or pole mounted, or building integrated). The proposed UST project may utilize the Master Plan’s solar energy standards to help comply with the Sustainable Building Ordinance, but the project is not required to provide solar energy systems.

- Ballfield Schedule and Usage.** The capacities for the proposed UST Ballfields include a maximum of 1,500 seats for baseball and 1,000 seats for softball. Regular practice for the UST baseball and softball teams begins September through October with approximately six total home games. Spring season starts when snow melt occurs, with approximately 20 total home games during regular season beginning the last week of March and ending in May. Post-season games may extend into early June. Games are primarily on Fridays, Saturdays, and Sundays with some on Tuesday and Wednesday evenings. UST expects the fields to be made available to high school and youth programs periodically; the timing of these games will vary.

Public Hearing Comments and Response

On March 17, 2023, the Planning Commission provided feedback on the proposed Master Plan amendments while simultaneously releasing this staff report for public review. On April 28, 2023, the Planning Commission held a Public Hearing on the proposed Master Plan amendments. Nine pieces of written testimony were provided in support of the proposed Master Plan amendments. The majority of the written testimony mentioned specific support for the approval of the Master Plan amendment to allow a surface parking lot instead a parking structure. Five members of the public provided verbal testimony in support of the proposed Master Plan amendments, four of which were from the project team (Ryan Companies and UST) and one of which was from Friends of Highland Ball. Key comments and staff responses are summarized below.

- **University of Saint Thomas Conditional Use Permit.** In Saint Paul, colleges and universities commonly have conditional use permits to allow for civic & institutional uses to be developed within a defined campus boundary. These conditional use permits are only required when campus development is proposed in a residential zoning district. The University of Saint Thomas has an approved conditional use permit allowing for civic & institutional uses associated with the contiguous campus to be developed within the existing R2 One-Family Residential Zoning District. The proposed UST development in Highland Bridge is not part of the contiguous, defined campus boundary of the university, and the project is not being proposed within a residential zoning district, so the existing conditional use permit does not apply nor need to be expanded. Additionally, the proposed civic & institutional use is allowed in the F6 Gateway Zoning District, so there is no need for the applicant to pursue an additional conditional use permit.
- **Travel Demand Management.** The Travel Demand Management (TDM) provisions are intended to reduce single occupancy vehicle trips and implement comprehensive policies for balance and choice in transportation options. Due to the size of this development (greater than 20,000 GFA of nonresidential use), a TDM Plan (TDMP) will be required during site plan review per Zoning Code Sec. 63.122. The TDMP will build upon the comprehensive traffic study conducted for the Highland Bridge site.
- **Maximum Parking Calculation.** The application includes a proposed Master Plan amendment that would add civic & institutional uses as a separate land use category in Master Plan Table 4.7 with maximum parking thresholds per Sec. 63.207 of the Zoning Code. This will help clarify the maximum parking allowed for civic & institutional uses associated with the proposed UST development as well as future proposals for civic & institutional uses in Highland Bridge. The proposed UST development primarily includes two small stadium complexes that are accessory to the overall institutional use; “stadium, sports arena” is a land use found within the commercial land use category of the Zoning Code. The Zoning Administrator will determine the most appropriate land use for calculating maximum parking during site plan review. Regardless of the land use determined most appropriate, the proposed amendment will provide additional clarity for this and future projects.

- **Community Use.** The UST development will be privately funded and owned by UST to support Division I athletic programs. The ballfields will be available for scheduled fee-based use by public and private groups when not in use by UST. The surface parking lot will be available for public use when not in use by UST or groups scheduled at the ballfields. The applicant is working on a legal document to memorialize UST's commitment to allow access to the ballfields and surface parking, which is anticipated for review by City Council in June 2023. Per Division I NCAA regulations, UST is not able to provide preferential treatment to any singular entity or person as related to potential impacts on prospective student-athletes and their eligibility.
- **Approval Process.** The Planning Commission will provide a recommendation to the City Council, who will make a decision on the proposed amendments. The Master Plan amendments are the first step in the process for approval of the proposed UST development. Other processes, including the AUAR update, platting, site plan review, parkland dedication and diversion, wetland mitigation, and airport overlay variance, are occurring simultaneously with this application. Upon approval of the Master Plan amendments and other concurrent processes, UST will continue private fundraising efforts required to advance the site plan and building design.

Recommendation

Comprehensive and Neighborhood Planning Committee recommends that Planning Commission recommends approval of the proposed *Ford Site Zoning and Public Realm Master Plan* amendments to City Council.

Attachments

1. Redline Master Plan Amendments
2. Redline Zoning Code Amendments
3. Ryan Companies Application and Materials
4. Written Testimony

The site shall be developed in a moderately dense, urban manner that reflects the historic pattern of mixed-use urban neighborhoods. Buildings are to be located toward the front of the lot, ranging in height from two (2) to ten (10) stories, and occupying much of the available lot area, with remaining space used efficiently for landscaping, small yard or common areas, parking access, and storage or waste facilities.

Table 4.1 Zoning District Summary

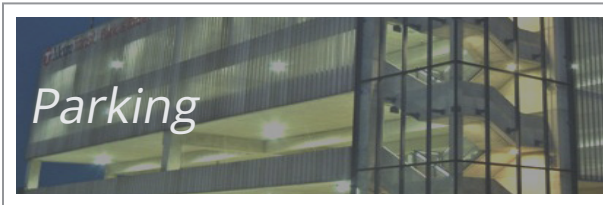
Zoning District	Description	Land Uses	Building Heights	Floor Area Ratio ⁴
F1 River Residential	High quality design and residential form that is compatible with the look of Mississippi River Blvd Boulevard	Residential mix of one-family dwellings, multi-unit homes, townhouses, and carriage houses	20 feet - Minimum 48 feet - Maximum	0.25 - 1.5
F2 Residential Mixed Low	Primarily residential with few business uses; lower density	Residential mix of primarily townhouses with some small multi-family	30 feet - Minimum 55 feet - Maximum	1.0 - 2.0
F3 Residential Mixed Mid	Primarily residential with some business uses; medium density	Predominantly multi-family residential, with limited retail, service and office	30 feet - Minimum 65 feet - Maximum (or 75 feet with stepback) ¹	1.0 - 4.0
F4 Residential Mixed High	Mix of residential and business uses; high density	Predominantly multi-family residential, with limited retail, service and office	48 feet - Minimum 75 feet - Maximum (or 110 feet with dedicated parkland) ²	3.0 - 6.0
F5 Business Mixed	Primarily retail, office and service with some multi-family residential	Primarily retail, service & office with some multi-family	40 feet - Minimum 65 feet - Maximum (or 75 feet with stepback) ³	2.0 - 4.0
F6 Gateway	Attractive gateways into site, focused on employment but with some retail, service, and residential	Office, institutional, retail & service; limited multi-family, and mixed residential & commercial	30 feet - Minimum 65 feet - Maximum	1.0 - 3.0

¹ Maximum height up to 75' with stepbacks per Table 66.931, note (c)

² Maximum height up to 110' with dedicated parkland per Table 66.931, note d)

³ Maximum height up to 75' per Table 66.931, note (e)

⁴ Civic and institutional buildings exempt from minimum FAR requirement per Table 66.931, note (j)



Purpose: to provide vehicular parking that meets the basic demand created by uses on the site, through the predominant use of structured parking. To provide convenient, plentiful and secure bicycle parking at places of residence, employment, shopping or service, and recreation.

Parking Standards are pursuant to Saint Paul Zoning Code, except as noted herein.

Table 4.7 Vehicle Parking Requirements by Use Category

Maximum Parking	
Land Use Categories	Maximum Spaces Allowed
Non-residential	1 space per 200 square feet GFA
Residential, dwellings	2 spaces per dwelling unit
Residential, congregate living	1 space per bedroom
<u>Civic & Institutional</u>	<u>See Zoning Code Sec. 63.207</u>

Note: There are no special provisions to exceed maximum required parking.

Surface parking shall not exceed 20 spaces per development block, except for civic and institutional uses in the F6 Zoning District east of Finn Street or south of Montreal Avenue.

Structured parking is permitted provided, that it meets the parking and design standards in the T3 district where arterial will mean primary street and collector will mean secondary street as defined in Ford Master Plan. Underground parking is permitted in any structure. Structured parking should be designed with level parking floors and adequate floor-to-ceiling clearance height to allow the space to be converted to finished floor area if parking is no longer needed in the future and such design is determined cost-effective.

Shared parking facilities are allowed and encouraged.

Residential Parking for 1-6 unit buildings: Parking garages for residential structures of one to six-units shall be placed at the rear of the building with access to a lane or alley or as a tuck under placed at least ten (10) feet behind the building facade. Tuck under parking on the main frontage of a residential structure is only permitted if there is no rear access to the lot from an alley or lane. Surface parking spaces or semi-covered spaces are allowed in lieu of garages, if placed at the rear of the building with access to a lane or alley. Driveways shall only provide access to the alley or lane at the rear of the lot, unless no such right-of-way exists.

For residential buildings on lots less than 50 feet wide and for all Sideyard, Rearyard, and Courtyard residential buildings, garages or off-street parking shall be accessed from an alley or via a shared driveway only.

G14. New buildings on corner lots shall be oriented to the corner and both public streets.

G15. For new Live/Work, Mixed Residential and Commercial, Civic and Institutional, and Commercial & Employment buildings, windows and doors or openings shall comprise at least fifty (50) percent of the length and at least thirty (30) percent of the area of the ground floor along addressed sides of the building and sides that face open space.

G16. Windows shall be designed with punched and recessed openings or other window installations that create a strong rhythm of light and shadow. Glass on windows and doors shall be clear or slightly tinted, and allow views into and out of the interior.

G17. If an outdoor storage, service or loading area is visible from adjacent residential uses, open spaces, or a public street or walkway, it shall be screened by a decorative fence, wall or screen of plant material at least six (6) feet in height.

G18. Surface parking shall not be located facing a front street or within thirty (30) feet of a corner.

G19. Exterior building materials shall consist of high-quality materials on all sides of buildings, such as brick, stone, tinted masonry or cast stone, stucco, glass, metal, and fiber cement cladding/siding. The following materials are not acceptable: unadorned plain or painted concrete block; tilt-up concrete panels; synthetic stucco products (EIFS); reflective glass; and vinyl, fiberglass, asphalt or fiberboard siding.

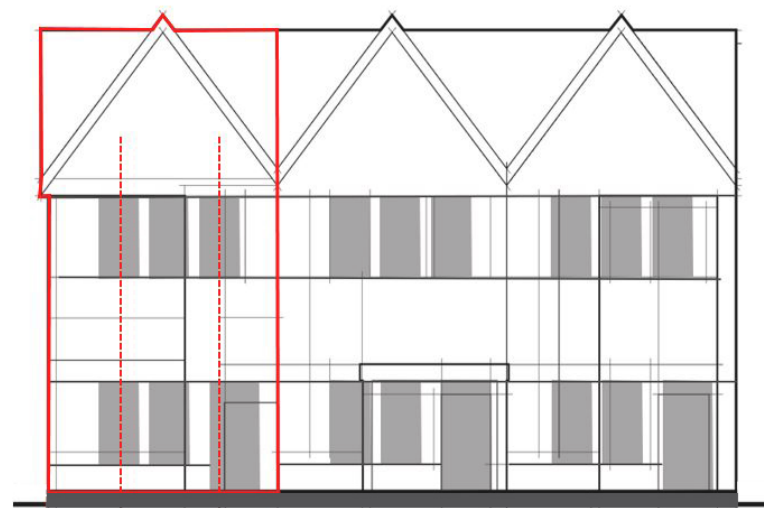
Standards specific to Multi-Unit & Townhouse Building Types

G20. Stoops may project into a required yard.

G21. Exterior stairs may be parallel or perpendicular to façade.

G22. Townhouse/rowhouse buildings shall exhibit vertical proportions; building elements including windows, balconies, doors, etc. shall reinforce the overall vertical proportions.

G23. All entrances within ten feet (10') of the front property line shall be raised above the average finished grade of the sidewalk a minimum of twelve inches (12") and a maximum of forty-eight inches (48") subject to ADA requirements.



Townhouse facade that exhibits vertical proportions

6.3 Building Type Standards

Zoning standards related to each building type are summarized in the table below. These standards are in addition to those related to the underlying zoning district. Subsequent pages address each building type individually, identifying their specific zoning standards and characteristics.

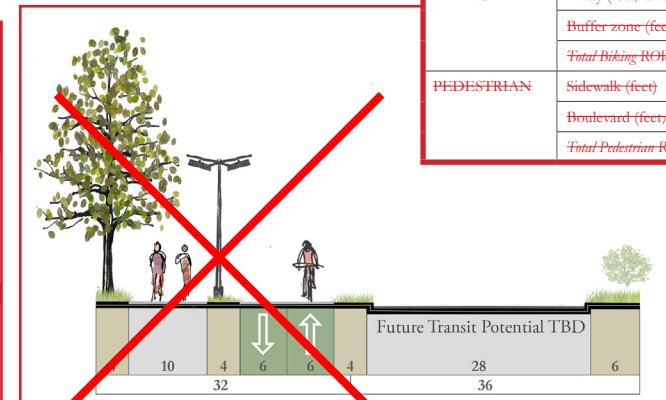
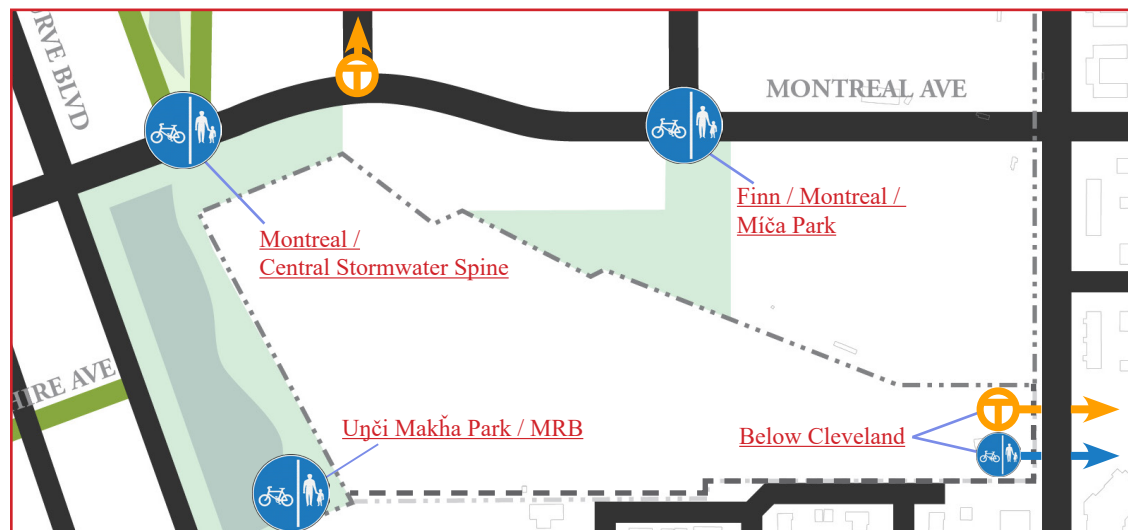
Table 6.2 Building Type Standards Summary Table

STANDARD	BUILDING TYPE									
	Single-Family Home	Multi-Unit Home	Carriage House	Townhouse / Rowhouse	Multi-Family	Live/Work	Mixed Residential & Commercial	Civic & Institutional	Commercial & Employment	Parking Structure
Units per Building	1	2-6	1-2	3-16	6+	2-8	n/a			
Building Width, maximum	60'			350'(e)	n/a	150'	500'			
Lot Width, minimum	60'	80'	Per requirement of primary structure	20'(b)	n/a	30'	n/a			
Lot Coverage by Buildings, maximum	40%		Included in coverage with primary structure	50%	70% (d)					
Building Height	Determined by Zoning District		30' maximum	Determined by Zoning District						
Public Right-of-Way Setback (a)	Min. = 10' Max. = 40'		Min. = 10' Max. = 20'			Min. = 5' Max. = 20'	Min. = 5' Max. = 15'			
Interior Lot Line Setback	Min. = 10' Max. = n/a		Min. = 6' (c) Max. = n/a							
Parking	Max. = 2.0 spaces per dwelling unit; Max. = 1.0 space per bedroom for congregate living					Use combined standards for residential and non-residential uses		Max. = 1.0 space per 200 square feet gross floor area		
Accessory Structures	Up to 3 including carriage house	Up to 1 per dwelling unit			Up to 2 per structure	Up to 1 per dwelling unit	Up to 2 per structure			

- (a) Maximum building setback limit shall apply to at least 60% of the building façade along the right-of-way. Buildings shall be setback a minimum of thirty (30) feet, with no maximum setback, from a lot line separating a lot from Mississippi River Boulevard. The minimum setback for a townhouse from a lot line along Beechwood, Saunders and Yorkshire Avenues shall be four (4) feet. Civic & Institutional buildings are exempt from the maximum setback requirement.
- (b) The minimum lot width figure for townhouses is per unit. Where land under each unit constitutes an individually described lot and all other land required for yards, parking and access constitutes “common” properties jointly owned by the owners of the units, the floor area ratio, lot width, and lot coverage requirements shall be applied to the entire parcel.
- (c) No setback is required for building walls containing no windows or other openings when the wall meets the fire resistance standards of the Minnesota State Building Code and there is a Common Interest Community (CIC) or recorded maintenance easement that covers the affected properties.
- (d) Portions of a parking structure that are less than one story above grade as defined in Section 60.208, and serve as amenity space shall be excluded from lot coverage by buildings in lot coverage calculations.
- (e) Maximum building width for townhouses on Mississippi River Boulevard is 150'

Shared Transportation Corridor(s)

The transportation network for the Highland Bridge site will include multiple corridors for walking, biking and transit. The alignment of the former Canadian Pacific Rail Spur (CP Spur) provides one such opportunity for one or more of these modes. While planning and final decisions about how to reposition the railway transit service will be determined in the coming years, the ability to connect multiple transportation modes through this area of the site shall be maintained. Connections serving both pedestrians and bicyclists between the CP Spur and key locations on the site (see below) shall be created. The ideal connection would be between the CP Spur and Mississippi River Boulevard (MRB) consistent in character with MRB (i.e., separated trails) and with minimal vehicular conflicts. Additional pedestrian and bicycle connections shall be created as development occurs and may need to be phased to avoid dead-end trails. The potential for transit service shall be maintained between the CP Spur and Cretin Avenue until the option is eliminated by the City and transit agencies. Final design and location of facilities will require additional engineering and City approval. The City must agree to the location(s) and legal mechanism for dedication (easement(s)) of the shared transportation corridor prior to final site plan approval. The right-of-way spacing would permit co-location of multiple modes as shown below. are not yet certain.



TOTAL RIGHT-OF-WAY		68
TRANSIT	2-way (feet/lane)	14
	Buffer zone (feet)	2,6
	Total Transit ROW	36
BIKING	2-way (feet/lane)	6
	Buffer zone (feet/side)	2
	Total Biking ROW	16
PEDESTRIAN	Sidewalk (feet)	10
	Boulevard (feet/side)	2,4
	Total Pedestrian ROW	16

- Potential Transit Connections to be Maintained
- Pedestrian and/or Bicycle Connections (Match design of existing or planned facilities)



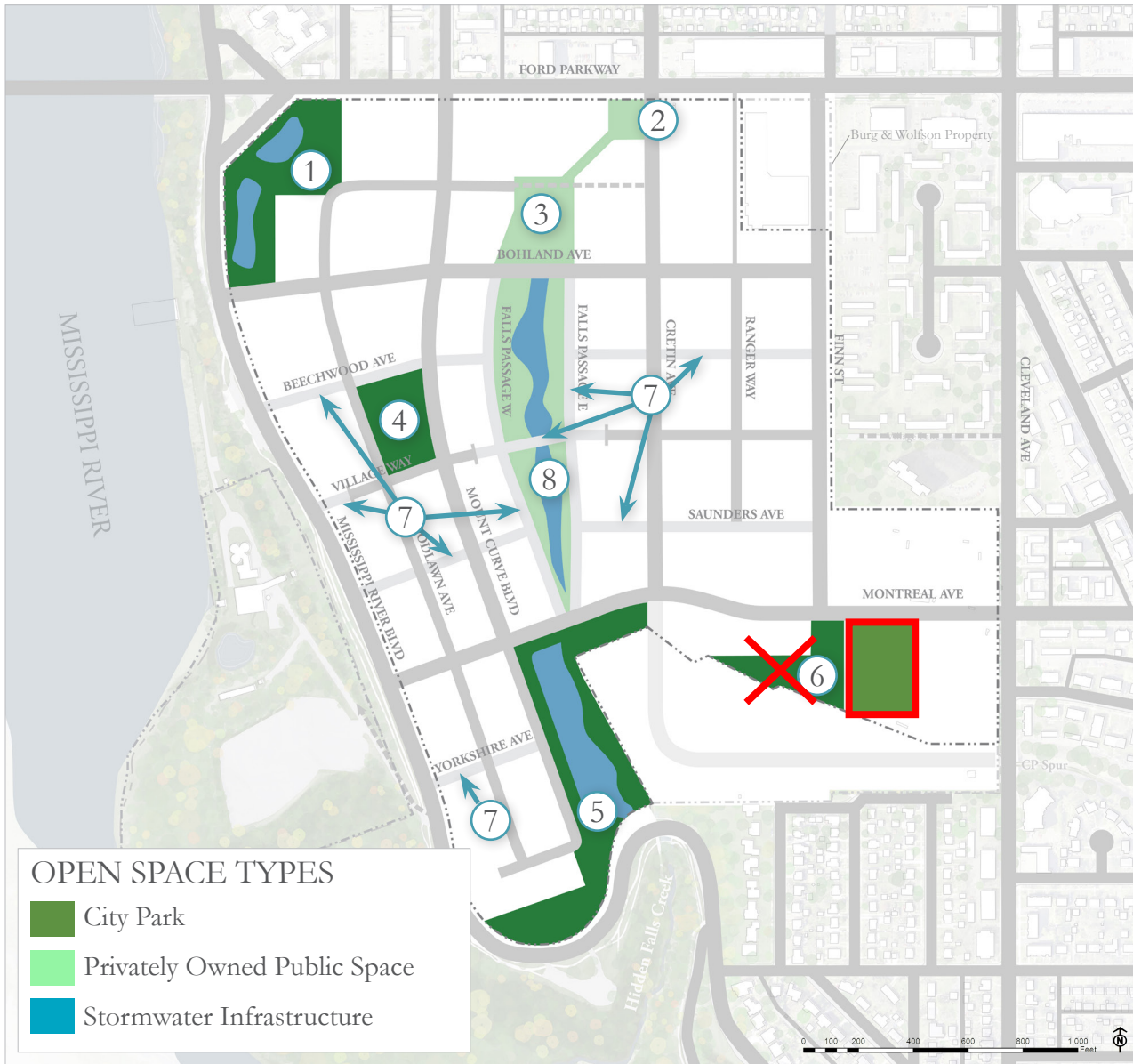
8.1 Introduction

Parks and open space at the Ford site are an essential element of a healthy, attractive and vibrant community. They improve the experience of people at the site and enhance value for adjacent and nearby users. Parks and open space expected at the site are categorized as one of six (6) major types:

- Gateway Park
- Urban Plaza and Civic Square
- Neighborhood Park
- Pocket Parks
- Hidden Falls Headwater Park
- Community Green Space
- Recreational Fields

Each type is outlined below in this chapter with a general description, location map, example images, and identifies key potential elements for the space. ~~Design and performance standards for specific elements are provided~~ Additional information can be found in the Ford Site Open Space Guidelines Report, found on the City's Ford web pages.

8.2 Open Space System



The open space system is intended to feel continuous in everyday experience. There are three categories of open space types. The first is the traditional city park, which is acquired through parkland dedication and would become part of the city’s park system. The second is stormwater infrastructure, which may feel like a park, but technically serves a utility function. Non-public recreation space, which is neither owned nor operated by the city, is an allowed use in all zoning districts.

OPEN SPACE ELEMENTS

- ① Gateway Park
- ② Urban Plaza
- ③ Civic Square
- ④ Neighborhood Park (*Assembly Union Park*)
- ⑤ Hidden Falls Headwaters (*Unčič Makha Park*)
- ⑥ Community Green Space (*Miča Park*)
- ⑦ Walking and Biking Paths
- ⑧ Central Stormwater Spine

This exhibit supersedes other exhibits that illustrate the open space system. Final size and location of open space elements and stormwater TBD based on future design and engineering, and subject to City approval.

Community Green Space (Míča Park)

The community green space will provide common areas to support future community gardens and other active and passive uses. ~~Steep slopes~~ ~~Wetlands~~ are present on the south side of this park, and the green space will include well-defined landscaped edges and ~~retaining walls for stability and access~~ other measures necessary for public safety and usability, and wetland mitigation. The final design of the park shall help connect surrounding uses to one another.

Potential Elements:

- Places to sit
- Play activity for kids, adults and/or seniors
- Attractive landscaping
- Small gardens or community garden plots
- Walkways/trails



Division 3. 66.930. Ford District Dimensional Standards

Sec. 66.931. Ford district dimensional standards table.

Table 66.931, Ford district dimensional standards, sets forth density and dimensional standards that are specific to Ford districts. These standards are in addition to the provisions of chapter 63, regulations of general applicability. Where an existing building does not conform to the following requirements, the building may be expanded without fully meeting the requirements as long as the expansion does not increase the nonconformity.

Table 66.931. Ford District Dimensional Standards

Building Type by Zoning District (a)	Floor Area Ratio Min.—Max (i)	Lot Width Min. (feet)	Building Width Max. (feet)	Building Height (feet)		Max. Lot Coverage by Buildings(f)	Building Setbacks (feet)(g)	
				Min.	Max.		ROW(h) Min.—Max.	Interior Min.
F1 river residential								
One-family dwelling	0.25 - 1.5	60	60	20	48	40%	10 - 40	10
Multi-unit home	0.25 - 1.5	80	60	20	48	40%	10 - 40	10
Townhouse, rowhouse	0.25—1.5 (b)	20 (b)	150	20	48	50% (b)	10—20	6 (i)
Carriage house	0.25 - 1.5	n/a	60	n/a	30	40%	10 - 20	6 (i)
F2 residential mixed low								
Townhouse, rowhouse	1.0 - 2.0	20 (b)	350	30	55	50% (b)	10 - 20	6 (i)
Multifamily	1.0 - 2.0	n/a	n/a	30	55	70%	10 - 20	6 (i)
Carriage house	1.0 - 2.0	n/a	60	n/a	30	per main building	10 - 20	6 (i)
Live/work	1.0 - 2.0	30	150	30	55	70%	5 - 20	6 (i)
Nonresidential or mixed	1.0 - 2.0	n/a	500	30	55	70%	5 - 15	6 (i)
F3 residential mixed mid								
Townhouse, rowhouse	1.0 - 4.0	20 (b)	350	30	65 (c)	50% (b)	10 - 20	6 (i)
Multifamily	2.0 - 4.0	n/a	n/a	40	65 (c)	70%	10 - 20	6 (i)

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(Supp. No. 122)

Live/work	2.0 - 4.0	30	150	40	65 (c)	70%	5 - 20	6 (i)
Nonresidential or mixed	2.0 - 4.0	n/a	500	40	65 (c)	70%	5 - 15	6 (i)
F4 residential mixed high								
Townhouse, rowhouse	3.0 - 6.0	20 (b)	350	48	75 (d)	50% (b)	10 - 20	6 (i)
Multifamily	3.0 - 6.0	n/a	n/a	48	75 (d)	70%	10 - 20	6 (i)
Live/work	3.0 - 6.0	30	150	48	75 (d)	70%	5 - 20	6 (i)
Nonresidential or mixed	3.0 - 6.0	n/a	500	48	75 (d)	70%	5 - 15	6 (i)
F5 business mixed								
Multifamily	2.0—4.0	n/a	n/a	40	65 (e)	70%	5—15	6 (i)
Nonresidential or mixed	2.0 - 4.0	n/a	500	40	65 (e)	70%	5 - 15	6 (i)
F6 gateway								
Nonresidential or mixed	1.0 - 3.0	n/a	500	30	65	70%	5 - 15	6 (i)

Min.— Minimum Max.— Maximum ROW— Public Right-of-Way n/a—not applicable

Notes to table 66.931, Ford district dimensional standards:

- (a) Building types are described and defined in Chapter 6 of the Ford Site Zoning and Public Realm Master Plan.
- (b) The minimum lot width figure for townhouses is per unit. Where land under each unit constitutes an individually described lot and all other land required for yards, parking and access constitutes "common" properties jointly owned by the owners of the units, the floor area ratio, lot width, and lot coverage requirements shall be applied to the entire parcel.
- (c) A maximum building height of seventy-five (75) feet may be permitted with a minimum ten (10) foot stepback from all minimum setback lines for all portions of the building above a height of twenty-five (25) feet.
- (d) All portions of a building above a height of twenty-five (25) feet shall be stepped back a minimum of ten (10) feet from all minimum setback lines. The maximum building height may exceed seventy-five (75) feet, to a maximum of one hundred ten (110) feet, subject to the following conditions:
 - (1) A minimum of one (1) acre of buildable land in the F1, F2, F3, and/or F4 districts shall have been dedicated or conveyed to the city for public use for parks, playgrounds, recreation facilities, trails, or open space, in excess of the amount of land required to be dedicated for parkland at the time of platting. Such dedication of the additional parkland must be consistent with the criteria for parkland dedication in section 69.511 and is subject to city council approval.

- (2) Maximum developable gross floor area of dedicated land from (c)(1), based on its underlying zoning, may be transferred and added to development allowed in an F4-zoned area, in compliance with other applicable requirements for the district or building, such as FAR, setbacks and open space coverage.
- (e) Building height may exceed sixty-five (65) feet, to a maximum of seventy-five (75) feet, with a minimum ten (10) foot stepback from all minimum setback lines for all portions of the building above a height of thirty (30) feet, except for corner elements and portions of the building facing the civic square identified in the Ford Site Zoning and Public Realm Master Plan, Chapter 8.
- (f) Portions of a parking structure that are less than one (1) story above grade, as defined in section 60.208, and serve as amenity space shall be excluded from lot coverage by buildings in lot coverage calculations.
- (g) Building setback is the horizontal distance between a lot line and the nearest above-grade point of a building. An interior setback is measured from an interior lot line, which is a lot line separating a lot from another lot or lots. A public right-of-way (ROW) setback is measured from a lot line that is not an interior lot line: a lot line separating a lot from a street, alley, or public way.
- (h) Maximum building setback shall apply to at least sixty (60) percent of the building facade along the right-of-way. Buildings shall be setback a minimum of thirty (30) feet, with no maximum setback, from a lot line separating a lot from Mississippi River Boulevard. The minimum setback for a townhouse from a lot line along Beechwood, Saunders and Yorkshire Avenues, shall be four (4) feet. Civic and institutional buildings are exempt from the maximum setback requirement.

[Discussion: the F6 district envisions a range of uses and building types that can serve as gateways into the Highland Bridge site. The variety of building types found with civic & institutional uses can contribute to the walkable, urban neighborhood without placing buildings close to the street in the same way mixed-use and multifamily buildings do in other F districts. Therefore, the requirement to place all buildings close to the street limits the building patterns possible with civic & institutional development and does not consider project scenarios such as recreational facilities, which require specific siting for sun angles and large plaza spaces for queuing of spectators. In other Saint Paul areas, institutional uses are commonly located in residential districts that have no maximum setback requirement (e.g., University of Saint Thomas (R2), Saint Catherine University (R3), Concordia Saint Paul (RT1)).

The proposed exemption would allow the proposed UST buildings and ballfields to be sited based on the unique requirements of this development program. The proposed exemption would also be appropriate for other civic & institutional uses in all F districts, not only in the F6 district. Similar to the FAR requirement, future small structures on public or private parkland would require a variance from this standard without the approval of the proposed exemption.]

- (i) No setback is required for building walls containing no windows or other openings when the wall meets the fire resistance standards of the Minnesota State Building Code and there is a Common Interest Community (CIC) or recorded maintenance easement that covers the affected properties.

(j) Civic and institutional buildings are exempt from the minimum FAR requirement.

[Discussion: FAR is a development regulation that establishes minimum requirements and maximum thresholds for building mass. FAR is defined as “[t]he total floor area of all buildings or structures on a zoning lot divided by the area of said lot” (Zoning Code Sec. 60.207). A minimum FAR ensures that a base level of development intensity is provided on a site and a maximum FAR limits the size of buildings in proportion to the site. For example, a FAR of 1.0 means that the gross square footage of the building may be equal to the gross square footage of the lot area (i.e., a 1:1 ratio).

Civic & institutional uses are often characterized by large open spaces between and around buildings in a campus-like setting, typically leading to low FARs. The low FAR usually found with civic & institutional uses is consistent with the description of the F6 Gateway zoning district (see Figure 5) as “attractive gateways to the site” and compatible with existing open spaces (Highland Ball, Gateway Park) and future parkland in the F6 district areas. The UST project proposed within the F6 district includes ballfields, plaza and outdoor gathering spaces, areas for circulation through the site, and parking to accommodate the parking demand of spectators and users of nearby spaces, all of which contribute to project vibrancy but do not allow the project to meet the minimum FAR requirement of 1.0. The proposal to exclude civic & institutional uses from the minimum FAR requirement is consistent with the F6 district, surrounding uses, and the typical development of civic & institutional buildings. Because of this, it is appropriate to exclude civic & institutional uses from the minimum FAR requirement in the F6 zoning district.

As build out of the Highland Bridge site continues, it will likely be desirable to consider building small structures on parkland to serve the needs of park users (e.g., shelters, restrooms, storage, etc.). These structures are considered civic & institutional uses. It would be beneficial to exclude civic & institutional uses from the minimum FAR requirements in all F districts, not only in the F6 district; without a sitewide exemption, those projects will require a variance.]

(Ord 17-40, § 1, 9-27-17; Ord 19-19, § 4, 4-10-19; Ord 19-54, § 1, 9-11-19; Ord 20-17, § 2, 5-13-20)

PART II - LEGISLATIVE CODE

TITLE VIII - ZONING CODE

Chapter 66. - Zoning Code—Zoning District Uses, Density and Dimensional Standards

ARTICLE IX. - 66.900. FORD DISTRICTS

Division 4. 66.940. Ford District Development Standards

Division 4. 66.940. Ford District Development Standards**Sec. 66.942. Ford district vehicle parking standards.**

Off-street parking shall be provided as follows. These requirements supersede the parking requirements in section 63.207.

Table 66.942. Vehicle Parking Requirements by Use

Land Use	Maximum Number of Parking Spaces
Residential, dwellings	2 spaces per dwelling unit
Residential, congregate living	1 space per bedroom
Nonresidential	1 space per 200 square feet GFA
<u>Civic and Institutional</u>	<u>See Zoning Code Sec. 63.207</u>

[Discussion: Maximum parking thresholds ensure that vehicular parking space does not negatively affect the built environment. The Master Plan provides maximum parking requirements by land use category for non-residential, residential, and residential congregate living based on gross floor area (GFA). Civic & institutional uses vary immensely (e.g., day care, college, public and private parks/playgrounds, recreation, religious institutions, etc.) and commonly include spaces for public gathering, which do not contribute to the calculation of GFA. Subjecting all civic & institutional uses to the same parking maximum as all non-residential uses does not adequately regulate nor acknowledge the variations possible within the land use category. The Zoning Code establishes parking maximums for a range of land uses within the civic & institutional land use category and is better suited to regulate maximum parking for this land use category in the Highland Bridge site.

The UST project includes large, activated spaces within and outside of buildings. Maximum allowed parking spaces for the outdoor ballfield stadiums would more appropriately be calculated by seat, anticipating spectator attendance, as addressed in the Zoning Code, rather than by the GFA of the indoor concourse. The proposed amendment allows for the maximum number of parking spaces to better align with anticipated stadium attendance associated with the UST development and the future redevelopment of the Highland Ballfields.]



TRANSMITTAL

TO Spencer Miller-Johnson
City of St. Paul
Planning and Economic Development
25 W. 4th Street, Suite 1400
Saint Paul, MN 55102

DATE 01-25-23 **JOB NO.** 700-685

RE Highland Bridge Master Plan Amendments – University of St. Thomas

WE ARE SENDING YOU

ATTACHED **UNDER SEPARATE COVER** **VIA** Electronic

SHOP DRAWINGS/SUBMITTALS SPECIFICATIONS PERMIT APPLICATIONS
 PLANS/PRINTS CHANGE ORDER CONTRACT
 SAMPLES COPY OF LETTER OTHER: _____

PURPOSE

FOR APPROVAL REVISE AND RESUBMIT FOR SIGNATURE
 FOR YOUR USE FOR FIELD USE AS REQUESTED
 APPROVED AS NOTED FOR REVIEW AND RESPONSE FOR BIDS DUE: _____
 APPROVED AS SUBMITTED FOR BUDGET PRICING

COPIES	DATE	NO.	DESCRIPTION
1	01/25/23	1	Cover Letter
1	Varies	2	Master Plan Amendment Application Form (F6)
1	Varies	3	Zoning Code Amendment Application Form (F6)
1	Varies	4	Master Plan Amendment Application Form (Full Dev)
1	Varies	5	Zoning Code Amendment Application Form (Full Dev)
1	01/25/23	6	Legal Descriptions & Property Calculations
1	01/25/23	7	Executive Summary
1	01/25/23	8	Amendments List
1	01/25/23	9	UST Ballfields Supplemental Memo
1	01/25/23	10	UST Ballifields Supplemental Images
1	TBD	11	Application Fee (not included in initial submittal)



COMMENTS: IF ENCLOSURES ARE NOT AS NOTED, KINDLY NOTIFY US AT ONCE.

Please find the items for the Highland Bridge Master Plan Amendment Submittal for the University of St. Thomas Ballfields Project. Note that the project team will follow up with the application fee and that the project team is working on additional imagery to share prior to the first CNPC meeting. Let us know if you need anything else!

Anthony Adams



January 25, 2023

Spencer Miller-Johnson
Senior City Planner
City of Saint Paul
25 W. 4th Street, Suite 1400
Saint Paul, MN 55102

RE: Highland Bridge Master Plan Amendments – University of St. Thomas

Dear Mr. Miller-Johnson:

Ryan Companies US, Inc. (Ryan) is applying for proposed amendments to the Ford Site Zoning and Public Realm Master Plan (Master Plan), adopted by the St. Paul City Council on 9/21/22. The intent of the amendment is to update various guidelines mainly within the F6 Gateway District and/or those that apply to Civic & Institutional type facilities in conjunction with the proposed University of St. Thomas (UST) Ballfields project. There are multiple co-applicants listed within the summary and applications.

Included in this submittal is the following information:

- **Application Forms** – four application forms for various levels of the Master Plan or Zoning Code amendments as some are pertinent only to the F6 district and the properties within that district, while one of the amendments relates to the broader Highland Bridge development.
- **Legal Descriptions & Property Calculations** – information included for the F6 zoning district along with the land ownership percentages for the full Highland Bridge development.
- **Executive Summary** – a summary to be used for the Master Plan and Zoning Code Amendments.
- **Amendments Lists** – excel spreadsheet that lists each proposed amendment, where it is found in the Master Plan, how that amendment affects the City of St. Paul zoning code/text/map, and the justification linked to each proposed amendment.
- **Supplemental Information** – memo and imagery to provide more detail on the proposed UST Ballfields project. Note that the project team is working on additional imagery and plans to follow up with that prior to the first CNCP meeting.

Ryan Companies US, Inc.
533 South Third Street, Suite 100
Minneapolis, MN 55415

p: 612-492-4000
ryancompanies.com



Please reach out if there are any questions or clarification with the documents.

Sincerely,

A handwritten signature in blue ink, which appears to read "Maureen Michalski". The signature is written in a cursive style and is contained within a light gray rectangular box.

Maureen Michalski
Vice President, Real Estate Development
Ryan Companies US, Inc.



MASTER PLAN AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
File #	_____
Fee Paid \$	_____
Received By / Date	_____
Tentative Hearing Date	_____

APPLICANT

Name	Ryan Companies US, Inc.	Email	maureen.michalski@ryancompanies.com
<i>(must have ownership or leasehold interest in the property, contingent included)</i>			
Address	533 S Third St, Suite 100	City	Minneapolis
		State	MN
		Zip	55415
Email	maureen.michalski@ryancompanies.com	Phone	612-492-4603
Name of Owner (if different)	_____	Email	_____
Contact Person (if different)	_____	Email	_____
Address	_____	City	_____
		State	_____
		Zip	_____

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(attach additional sheet if necessary)</i>
Lot Area	38.95 acres
Current Zoning	F6

REQUEST: Application is hereby made to modify or amend an adopted Master Plan under provisions of Zoning Code § 66.344(c) or § 66.951.

Proposed Amendment Amendments to the Ford Site Zoning and Public Realm Master Plan. See Amendments List document.
(attach additional sheets if necessary)

SUPPORTING INFORMATION: State what the Master Plan calls for, what amendment is proposed, and the reason for the amendment. Attach additional sheets if necessary.

Ryan is the main applicant for all Master Plan amendment requests within the F6 District as the master developer of Highland Bridge and as having ownership in property that is subject to the proposed modifications.

St. Paul Parks Dept and Capital City Properties are co-applicants for the Shared Transportation Corridor and Open Space System amendment requests as they have ownership in property that is subject of the proposed modifications.

St. Paul Parks Dept, Capital City Properties, and Friends of Highland Ball are co-applicants for the Surface Parking amendment request as they have ownership in property that is subject of the proposed modification.

- Required site plan is attached.
- If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

DocuSigned by:
Applicant's Signature *Zachary Ryan* **Date** 1/25/2023

908A132EC8DE42E
Friends of Highland Ball

Treasurer



MASTER PLAN AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
File #	_____
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Tentative Hearing Date	_____

APPLICANT

Name	Ryan Companies US, Inc.	Email	maureen.michalski@ryancompanies.com		
<i>(must have ownership or leasehold interest in the property, contingent included)</i>					
Address	533 S Third St, Suite 100	City	Minneapolis	State	MN Zip 55415
Email	maureen.michalski@ryancompanies.com		Phone	612-492-4603	
Name of Owner (if different)	_____		Email	_____	
Contact Person (if different)	_____		Email	_____	
Address	_____	City	_____	State	_____ Zip _____

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116				
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(attach additional sheet if necessary)</i>				
Lot Area	38.95 acres	Current Zoning	F6		

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St. Paul Parks Dept, St. Paul Port Authority, and Highland Ball are co-applicants for the Surface Parking amendment request as they have ownership in property that is subject of the proposed modification.

- Required site plan is attached.
- If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

DocuSigned by:  **Applicant's Signature** _____ **Date** 1/19/2023

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MASTER PLAN AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
File #	_____
Fee Paid \$	_____
Received By / Date	_____
Tentative Hearing Date	_____

APPLICANT

Name	Ryan Companies US, Inc.	Email	maureen.michalski@ryancompanies.com
<i>(must have ownership or leasehold interest in the property, contingent included)</i>			
Address	533 S Third St, Suite 100	City	Minneapolis
		State	MN
		Zip	55415
Email	maureen.michalski@ryancompanies.com	Phone	612-492-4603
Name of Owner (if different)	_____	Email	_____
Contact Person (if different)	_____	Email	_____
Address	_____	City	_____
		State	_____
		Zip	_____

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(attach additional sheet if necessary)</i>
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Current Zoning	F6

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- Required site plan is attached.
- If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

DocuSigned by:
Applicant's Signature eSarah Illi **Date** 1/23/2023

F0CFA45AEF6C462...

Capital City Properties

Director of Development

Rev 8.5.2019



MASTER PLAN AMENDMENT APPLICATION

Department of Planning and Economic Development
 Zoning Section
 1400 City Hall Annex, 25 West Fourth Street
 Saint Paul, MN 55102-1634
 (651) 266-6583

Zoning Office Use Only

File # _____

Fee Paid \$ _____

Received By / Date _____

Tentative Hearing Date _____

APPLICANT

Name Ryan Companies US, Inc. Email maureen.michalski@ryancompanies.com
(must have ownership or leasehold interest in the property, contingent included)

Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Name of Owner (if different) _____ Email _____

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(attach additional sheet if necessary)

Lot Area 38.95 acres Current Zoning F6

REQUEST: Application is hereby made to modify or amend an adopted Master Plan under provisions of Zoning Code § 66.344(c) or § 66.951.

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- If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

DocuSigned by: Andy Rodriguez Date 1/24/2023

Applicant's Signature

021B683837F046F
 Saint Paul Parks and Recreation

Parks and Recreation Director
 Rev 8.5.2019



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
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Tentative Hearing Date	_____

APPLICANT

Property Owner(s) Ryan Companies US, Inc.

Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(Attach additional sheet if necessary.)

_____ Lot Area 38.95 acres Current Zoning F6

TO THE HONORABLE MAYOR AND CITY COUNCIL:

Pursuant to Saint Paul Zoning Code § 61.801 and Minnesota Statutes § 462.357, Project Paul, LLC (Ryan),
Capital City Properties (CCP), and Friends of Highland Ball (Highland Ball)

owner(s) of at least 67% of the land in the F6 zoning district(s), hereby petition(s) to amend provisions in the Zoning Code pertaining to the F6 zoning district(s) as follows:

Floor Area Ratio and Building Standards Amendments. See attached Executive Summary and Amendments List

Attach additional sheets if necessary. Attachments as required: Site Plan

If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: Zachary Ryan
Fee owner of property
Title: Treasurer
Friends of Highland Ball

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

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Tentative Hearing Date	_____

APPLICANT

Property Owner(s) Ryan Companies US, Inc.

Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(Attach additional sheet if necessary.)

_____ Lot Area 38.95 acres Current Zoning F6

TO THE HONORABLE MAYOR AND CITY COUNCIL:

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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By:
E145...
Title: Vice President

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

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Tentative Hearing Date	_____

APPLICANT

Property Owner(s) Ryan Companies US, Inc.

Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(Attach additional sheet if necessary.)

_____ Lot Area 38.95 acres Current Zoning F6

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Pursuant to Saint Paul Zoning Code § 61.801 and Minnesota Statutes § 462.357, Project Paul, LLC (Ryan),
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Attach additional sheets if necessary. Attachments as required: Site Plan

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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: eSarah Illi
Fee owner of property
Title: Director of Development
Capital City Properties

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



MASTER PLAN AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
File #	_____
Fee Paid \$	_____
Received By / Date	_____
Tentative Hearing Date	_____

APPLICANT

Name	Ryan Companies US, Inc.	Email	maureen.michalski@ryancompanies.com				
<i>(must have ownership or leasehold interest in the property, contingent included)</i>							
Address	533 S Third St, Suite 100	City	Minneapolis	State	MN	Zip	55415
Email	maureen.michalski@ryancompanies.com	Phone	612-492-4603				
Name of Owner (if different)	_____	Email	_____				
Contact Person (if different)	_____	Email	_____				
Address	_____	City	_____	State	_____	Zip	_____

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116		
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(attach additional sheet if necessary)</i>		
Lot Area	108.75 acres	Current Zoning	F1-F6

REQUEST: Application is hereby made to modify or amend an adopted Master Plan under provisions of Zoning Code § 66.344(c) or § 66.951.

Proposed Amendment Amendment to the Ford Site Zoning and Public Realm Master Plan. See Amendments List document.
(attach additional sheets if necessary)

SUPPORTING INFORMATION: State what the Master Plan calls for, what amendment is proposed, and the reason for the amendment. Attach additional sheets if necessary.

Ryan is the applicant for the Parking Modification Master Plan amendment request as the master developer of the Highland Bridge Development.

Required site plan is attached.

If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

DocuSigned by: Date 1/19/2023

Applicant's Signature _____ Date _____

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ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

Zoning Office Use Only	
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APPLICANT

Property Owner(s) Ryan Companies US, Inc.

Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(Attach additional sheet if necessary.)

_____ Lot Area 108.75 acres Current Zoning F1-F6

TO THE HONORABLE MAYOR AND CITY COUNCIL:

Pursuant to Saint Paul Zoning Code § 61.801 and Minnesota Statutes § 462.357, Project Paul, LLC (Ryan),
Capital City Properties (CCP), Friends of Highland Ball (Highland Ball), St. Paul Parks Dept (Parks), and Weidner Apartment Homes (Weidner)

owner(s) of at least 67% of the land in the F1-F6 zoning district(s), hereby petition(s) to amend provisions in the Zoning Code pertaining to the F1-F6 zoning district(s) as follows:

Parking Modification Amendment. See attached Executive Summary and Amendments List

Attach additional sheets if necessary. Attachments as required: Site Plan

If you are a religious institution you may have certain rights under RLUIPA. Please check this box if you identify as a religious institution.

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: Eadhary Ryan
948325 owner of property

Title: Treasurer
Friends of Highland Ball

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

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APPLICANT

Property Owner(s)	Ryan Companies US, Inc.							
Address	533 S Third St, Suite 100	City	Minneapolis	State	MN	Zip	55415	
Email	maureen.michalski@ryancompanies.com		Phone	612-492-4603				
Contact Person (if different)	_____		Email	_____				
Address	_____	City	_____	State	_____	Zip	_____	

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116						
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(Attach additional sheet if necessary.)</i>						
Lot Area	108.75 acres	Current Zoning	F1-F6				

TO THE HONORABLE MAYOR AND CITY COUNCIL:

Pursuant to Saint Paul Zoning Code § 61.801 and Minnesota Statutes § 462.357, Project Paul, LLC (Ryan),
City of St. Paul Port Authority (Port), Friends of Highland Ball (Highland Ball), St. Paul Parks Dept (Parks), and Weidner Apartment Homes (Weidner)
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Attach additional sheets if necessary. Attachments as required: Site Plan

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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me


Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:

 By: _____
 E11A0C9DF3A8E18A
 Fee owner of property
 Title: Vice President
 By: _____
 Fee owner of property
 Title: _____
 By: _____
 Fee owner of property
 Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
(651) 266-6583

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Address 533 S Third St, Suite 100 City Minneapolis State MN Zip 55415

Email maureen.michalski@ryancompanies.com Phone 612-492-4603

Contact Person (if different) _____ Email _____

Address _____ City _____ State _____ Zip _____

(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)

PROPERTY INFO

Address/Location 2192 Ford Pkwy St. Paul, MN 55116

PIN(s) & Legal Description See attached for PINs & Legal Descriptions
(Attach additional sheet if necessary.)

_____ Lot Area 108.75 acres Current Zoning F1-F6

TO THE HONORABLE MAYOR AND CITY COUNCIL:

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Capital City Properties (CCP), Friends of Highland Ball (Highland Ball), St. Paul Parks Dept (Parks), and Weidner Apartment Homes (Weidner),

owner(s) of at least 67% of the land in the F1-F6 zoning district(s), hereby petition(s) to amend provisions in the Zoning Code pertaining to the F1-F6 zoning district(s) as follows:

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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: eSarah Illi
Fee owner of property

Title: Director of Development
Capital City Properties

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
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APPLICANT

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Address	533 S Third St, Suite 100	City	Minneapolis	State	MN	Zip	55415	
Email	maureen.michalski@ryancompanies.com		Phone	612-492-4603				
Contact Person (if different)	_____		Email	_____				
Address	_____	City	_____	State	_____	Zip	_____	
<i>(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)</i>								

PROPERTY INFO

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Lot Area	108.75 acres	Current Zoning	F1-F6				

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owner(s) of at least 67% of the land in the F1-F6 zoning district(s), hereby petition(s) to amend provisions in the Zoning Code pertaining to the F1-F6 zoning district(s) as follows:

Parking Modification Amendment. See attached Executive Summary and Amendments List

Attach additional sheets if necessary. Attachments as required: Site Plan

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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: Andy Rodriguez
Fee owner of property

Title: Parks and Recreation Director
Saint Paul Parks and Recreation

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



ZONING CODE AMENDMENT APPLICATION

Department of Planning and Economic Development
Zoning Section
1400 City Hall Annex, 25 West Fourth Street
Saint Paul, MN 55102-1634
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Property Owner(s)	Ryan Companies US, Inc.							
Address	533 S Third St, Suite 100	City	Minneapolis	State	MN	Zip	55415	
Email	maureen.michalski@ryancompanies.com		Phone	612-492-4603				
Contact Person (if different)	_____		Email	_____				
Address	_____	City	_____	State	_____	Zip	_____	
<i>(Attach additional sheet if necessary to include all of the owners of at least 67% of the property affected by the amendment.)</i>								

PROPERTY INFO

Address/Location	2192 Ford Pkwy St. Paul, MN 55116						
PIN(s) & Legal Description	See attached for PINs & Legal Descriptions <i>(Attach additional sheet if necessary.)</i>						
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Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

Subscribed and sworn to before me

Date _____ 20____

Notary Public

DocuSigned by:
By: Louisa Torchia
Fee owner of property
Title: Vice Pres. of Operations
Weidner Apartment Homes

By: _____
Fee owner of property

Title: _____

By: _____
Fee owner of property

Title: _____



Property Calculation – F6 District

Ownership Percentage - F6 District		
Owner	Parcel Area (acres)	Percentage
Capital City Properties (CCP)	12.73	32.68%
Ryan	9.77	25.08%
City of St. Paul (City)	8.24	21.16%
Highland Ball	5.15	13.22%
Presbyterian Homes & Services (PHS)	2.30	5.91%
Project for Pride in Living (PPL)	0.76	1.95%
Total	38.95	100.00%

70.99%

Property Identification Numbers (PINs) – F6 District

Park A (City): 172823210006
 Lot 1, Block 1 (Ryan): 172823120088
 Lot 2, Block 1 (PPL): 172823120092
 Lot 2, Block 1 (PPL): 172823120091
 Lot 1, Block 5 (Ryan): 172823210007
 Lot 1, Block 6 (PHS): 172823120084
 Park C (City): 172823420011
 Park D (City): 172823410078
 Lot 1, Block 30 (Highland Ball): 172823140012
 Lot 1, Block 33 (Ryan): 172823410076
 Lot 1, Block 34 (Ryan): 172823410077
 CP Rail Parcels (CCP): 172823410001 & 172823410002

Legal Descriptions – F6 District

Legal Description – Capital City Properties (CCP) Parcels

Parcel A1: Lot 2, Block 1, Ford Motor Company First Addition, Ramsey County, Minnesota.

(TORRENS PROPERTY, Certificate of Title No. 649087)

Parcel A2: That part of Lot 3, Block 1, Ford Motor Company First Addition lying southerly of the plat of Ford, Ramsey County, Minnesota.

(TORRENS PROPERTY, Certificate of Title No. 649088)

Parcel B: That part of Lot 1, Auditor's Subdivision No. 87 lying southerly of the South line of Lot 3, Block 1, Ford Motor Company First Addition, Ramsey County, Minnesota.

(TORRENS PROPERTY, Certificate of Title No. 649088)

Legal Description – Project Paul (Ryan) Parcels

Lot 1, Block 5;

Lot 1, Block 33;

Lot 1, Block 34;

all in FORD, according to the recorded plat thereof, Ramsey County, Minnesota

Legal Description – Friends of Highland Ball (Highland Ball) Parcels

Lot 1, Block 30, FORD, according to the recorded plat thereof, Ramsey County, Minnesota.

Legal Description – City of St. Paul Parks Department (Parks) Parcels

Park A;

Park C;

Park D;

all in FORD, according to the recorded plat thereof, Ramsey County, Minnesota.

Legal Description – Presbyterian Homes & Services Parcels

Lot 1, Block 6, FORD, according to the recorded plat thereof, Ramsey County, Minnesota.

Legal Description – Project Paul MOB (Ryan) Parcels

Parcel A: That part of Lot 1, Block 1, FORD, according to the recorded plat thereof, Ramsey County, Minnesota, lying northerly and westerly of the following described line: Commencing at the northeast corner of said Lot 1; thence South 00 degrees 04 minutes 33 seconds East, along the east line of said Lot 1, a distance of 138.18 feet to the point of beginning of the line to be described; thence South 89 degrees 55 minutes 26 seconds West, a distance of 76.00 feet; thence South 00 degrees 04 minutes 34 seconds East, a distance of 57.00 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 301.87 feet to the west line of said Lot 1 and said line there terminating.

Legal Description – Emma Norton PPL Limited Partnership (PPL)

That part of Lot 1, Block 1, FORD, according to the recorded plat thereof, Ramsey County, Minnesota, lying southerly and easterly of the following described line: Commencing at the northeast corner of said Lot 1; thence South 00 degrees 04 minutes 33 seconds East, along the east line of said Lot 1, a distance of 138.18 feet to the point of beginning of the line to be described; thence South 89 degrees 55 minutes 26 seconds West, a distance of 76.00 feet; thence South 00 degrees 04 minutes 34 seconds East, a distance of 57.00 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 301.87 feet to the west line of said Lot 1 and said line there terminating.

Except that part lying westerly of the following described line:

Commencing at the northeast corner of said Lot 1; thence South 00 degrees 04 minutes 33 seconds East, along the east line of said Lot 1, a distance of 138.18 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 76.00 feet; thence South 00 degrees 04 minutes 34 seconds East, a distance of 57.00 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 63.46 feet to the point of beginning of the line to be described; thence South 00 degrees 00 minutes 00 seconds East, a distance of 26.31 feet; thence North 90 degrees 00 minutes 00 seconds East, a distance of 8.08 feet; thence South 00 degrees 00 minutes 00 seconds East, a distance of 49.68 feet to the south line of said Lot 1 and said line there terminating.

Legal Description – Nellie Francis Court Limited Partnership (PPL)

That part of Lot 1, Block 1, FORD, according to the recorded plat thereof, Ramsey County, Minnesota, lying Southerly and Easterly of the following described line:

Commencing at the Northeast corner of said Lot 1; thence South 00 degrees 04 minutes 33 seconds East, along the East line of said Lot 1, a distance of 138.18 feet to the point of beginning of the line to be described; thence South 89 degrees 55 minutes 26 seconds West, a distance of 76.00 feet; thence South 00 degrees 04 minutes 34 seconds East, a distance of 57.00 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 301.87 feet to the west line of said Lot 1 and said line there terminating.

Except that part lying Easterly of the following described line:

Commencing at the Northeast corner of said Lot 1; thence South 00 degrees 04 minutes 33 seconds East, along the East line of said Lot 1, a distance of 138.18 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 76.00 feet; thence South 00 degrees 04 minutes 34 seconds East, a distance of 57.00 feet; thence South 89 degrees 55 minutes 26 seconds West, a distance of 63.46 feet to the point of beginning of the line to be described; thence South 00 degrees 00 minutes 00 seconds East, a distance of 26.31 feet; thence North 90 degrees 00 minutes 00 seconds East, a distance of 8.08 feet; thence South 00 degrees 00 minutes 00 seconds East, a distance of 49.68 feet to the South line of said Lot 1 and said line there terminating.

Property Calculation – Full Development

Ownership Percentage - Full Development		
Owner	Parcel Area (acres)	Percentage
Ryan	32.94	30.29%
Weidner	19.18	17.64%
City of St. Paul (City)	13.58	12.49%
Capital City Properties (CCP)	12.73	11.71%
Pulte Homes (Pulte)	8.34	7.67%
Highland Bridge Master Association (HBMA)	7.52	6.91%
Highland Ball	5.15	4.74%
Presbyterian Homes & Services (PHS)	3.60	3.31%
HV Shopping Center (HVSC)	3.76	3.46%
Project for Pride In Living (PPL)	0.76	0.70%
CommonBond Communities (CBC)	0.53	0.49%
Keller/Orchard (K/O)	0.18	0.17%
Heegard	0.17	0.16%
Fiocco/Sellew (F/S)	0.16	0.15%
Camilleri	0.15	0.14%
Total	108.75	100.00%

76.86%



Ford Site Zoning and Public Realm Master Plan

Ryan Companies Amendments – Executive Summary

01.25.2023

The following is an Executive Summary of the proposed changes to the Ford Site Zoning and Public Realm Master Plan (Master Plan). The proposed changes within this Summary include changes that require City Council action for approval. Some are considered Major Amendments to the Master Plan. Others are considered Minor Amendments to the Master Plan and would require a change to the City of St. Paul Zoning Code.

The definition of a major amendment allows for initiation by the city council, the planning commission, or any person having an ownership or leasehold interest (contingent included) in property that is the subject of the proposed modification. This is confusing and may be interpreted that all property owners must be on an application if it were a change to a regulation that would apply to the entire Highland Bridge development. The Highland Bridge development currently has 16 different owners or ownership groups and will soon have many more, likely approaching 400 different owners by the end of the development. It is not feasible to amend the Master Plan in the future if an application is needed from every owner. Ryan requests that City staff review this definition for process adjustments in the future.

Similarly, amendments to the zoning code require that owners of at least 67% of the property affected by the amendment be included in the zoning code amendment application. As mentioned above, this will not be feasible to amend the zoning code specific to the Ford Site zoning districts when that many ownership groups need to be within the application. Ryan requests that City staff review this requirement for process adjustments in the future.

The changes within this document are requested by Ryan Companies (Ryan) on behalf of the University of St. Thomas (UST) who is proposing to locate new Ballfields in the southeast portion of the Ford Site, now called Highland Bridge. The UST Ballfields will include baseball and softball facilities with a supporting practice building and parking on four current parcels of land including Block 33, Block 34, and Park D of the FORD plat, and the vacant parcels commonly known as the CP Rail parcels. This Summary may be used in conjunction with the Amendments List for a comprehensive assessment of required items. Additional information specific to the UST Ballfields project is provided in a supplemental memo and imagery.

There are multiple co-applicants within this submittal. Master Plan amendment requests that apply to specific land parcels within the F6 district and/or only to the F6 district include the following applicants:

- Ryan is the main applicant for all Master Plan amendment requests within the F6 District as the master developer of Highland Bridge and as having ownership in property that is subject to the proposed modifications.
- The City of Saint Paul Department of Parks and Recreation (SPPR), as the owner of Park D (Míča Park), and Capital City Properties (CCP), as the owner of the sites commonly known as the CP Rail parcels, are co-applicants for the Shared Transportation Corridor and Open Space System amendment requests as they have ownership in property that is subject of the proposed modifications.

- SPPR, CCP and the Friends of Highland Ball (Highland Ball), as the owner of Block 30, are co-applicants for the Surface Parking amendment request as they have ownership in property that is subject of the proposed modification.

Master Plan amendment requests that apply to the full development include the following applicants:

- Ryan is the only applicant as the master developer of Highland Bridge for the Parking Modification amendment request.

Zoning Code amendment requests that apply to the F6 district must have 67% ownership of the F6 district, so the applicants include:

- Ryan, CCP, and Highland Ball for the Floor Area Ratio and Building Standards amendment requests.

Zoning Code amendment requests that apply to the full development must have 67% ownership of the full development, so the applicants include:

- Ryan, CCP, Highland Ball, Parks, Weidner for the Parking Modification amendment request.

The following categories provide an overview of the proposed changes to the Ford Site Zoning and Public Realm Master Plan:

1. Floor Area Ratio (FAR)

The Master Plan requires a range of Floor Area Ratio, or FAR, for each of the six zoning districts ranging from 0.25 in the F1 district up to 6.0 in the F4 district. According to the Master Plan, this requirement establishes the overall mass of buildings on a property and is used to complement or in place of other lot and building standards such as height, lot coverage and setbacks.

a. Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses.

- i. The Master Plan allows for certain Civic & Institutional uses within the Ford Site zoning districts including colleges/universities, day cares, public and private parks/playgrounds, recreation, and religious institutions to name a few. These types of facilities commonly include spaces of public gathering or activation such as ballfields, outdoor play areas, and plazas which do not contribute to the floor area used in the Floor Area Ratio calculation but play a pivotal role in the operation of those uses. Often Civic & Institutional facilities are part of an overall campus design where there are broader goals for building placement and density and are also often only one to two level buildings, which further reduces the likelihood of meeting the minimum Floor Area Ratio requirements listed within the zoning districts.
- ii. For the UST project proposed within the F6 district, the project includes ballfields, plaza and outdoor gathering spaces, areas for circulation through the site, and parking to accommodate the parking demand of the project, all of which contribute to project vibrancy and activation, but do not allow the project to meet the minimum FAR requirement of 1.0. This exclusion would benefit the Highland Ballfields, also located within the F6

district, if those were to ever be renovated with new facilities in the future as well.

2. Parking

The Master Plan provides maximum parking requirements by land use category for non-residential, residential, and residential congregate living based on Gross Floor Area (GFA). The Master Plan also restricts the amount of surface parking stalls allowed to 20 surface stalls per development block.

a. **Modification to Table 4.7 for Civic & Institutional uses as a land use category and setting the maximum spaces allowed as provided in Section 63.207 of the Zoning Code**

- i. Civic & Institutional uses commonly include spaces of public gathering or activation such as ballfields, outdoor play areas, and plazas which do not contribute to the floor area used in the GFA calculation. With the broad non-residential land use category of Table 4.7 of the master plan, the maximum parking stalls calculation for Civic & Institutional uses does not align with the nature of the facilities, which have lesser quantity of buildings (and therefore GFA) but maintain a high demand for parking for attendees and visitors. Saint Paul Zoning Code Section 63.207 allows for Civic & Institutional uses, such as Sports Arenas, to calculate the maximum parking a different way than GFA, such as the number of seats within the Sports Arena. This amendment is requested to provide flexibility for Civic & Institutional uses to set the appropriate number of maximum parking stalls based on the specific use and as exemplified elsewhere in the Saint Paul Zoning Code.
- ii. For the UST project proposed, the project team is looking to phase portions of the buildings to be built at a future date subject to fundraising efforts. However, the spectator seats for the Ballfields are anticipated to be built in full with the initial project build. If the maximum parking ratio is set off building square footage, then the amount of parking stalls for spectators is severely limited to below what is adequate to serve the facilities. This amendment will allow for the maximum number of parking stalls to fit attendance within the arenas based on their capacity, i.e. number of seats, instead of limiting it based on the amount of building square footage. This exclusion would benefit the Highland Ballfields if those were to ever be renovated with new facilities in the future as well.

b. **Exclusion to the limitation of surface parking spaces within the southeasterly F6 district for Civic & Institutional uses.**

- i. Civic & Institutional uses, such as recreational facilities, are often part of an overall campus design where there are broader goals for building placement and site layout compared to the Ford Site Master Plan, which was set up based on an urban development and street grid layout indicated in areas other than the southeast F6 district. The Civic & Institutional uses planned within the southeasterly F6 district are proposed to follow more recreation specific layouts with larger building setbacks and different ways of accessing the sites.
- ii. For the UST project proposed within the F6 district, the project team has contemplated how a surface parking lot proposed could be shared with

the adjacent developments such as the parkland and Block 34 development, as well as providing flexibility for those adjacent developments as they get built out in the future. This includes the shared transportation corridor that is planned to route through the CP Rail parcel. There are other contextual benefits to the surface parking lot such as limiting parking views from the adjacent neighbors to the south of the site and limiting buildings within the Airport Overlay district. This exclusion would benefit the Highland Ballfields, also located within the southeasterly F6 district, if those were to ever be renovated with new facilities in the future as well.

3. Building Type Standards

The Master Plan includes standards specific to certain building types allowed within the overall development to guide for a wide range of uses, and to provide consistency in built form related to use, as opposed to geography.

a. Exclusion to the maximum Public Right of Way Setback within the F6 district for Civic & Institutional uses.

- i. The maximum public right of way setback is provided to hold the urban edge of the buildings to the right of way spaces. This regulation did not consider Civic & Institutional spaces such as colleges and universities, public and private parks, and recreational areas where those facilities are often intentionally set back from the public right of way to provide a buffer from the active use of the space or because they have other larger planning goals such as a university campus. There are also other needs to consider when locating Civic & Institutional uses on a site, such as sun angles and safety for ballfields, which determine the direction the fields face, entry areas for college and religious facilities for queuing of visitors, or public safety surrounding recreational uses.
- ii. The UST project has multiple components to the project design where prioritization of the following is driving buildings to exceed the maximum public right of way setback:
 - i. Safety for athletes (rotation of ballfields to accommodate sun angles)
 - ii. Safety for people utilizing the public right of way (foul balls)
 - iii. Plaza areas for public engagement
 - iv. Areas for entry and exit to the facilities (for adequate pedestrian movements)
 - v. Campus relationship between the various facilities

4. Shared Transportation Corridor

The Master Plan includes an opportunity to connect Highland Bridge to the former CP Rail corridor east of Cleveland Avenue for various modes of transportation such as walking, biking, and transit. A conceptual connection from the Cretin Ave and Montreal Ave intersection to the underside of the existing Cleveland Ave bridge was drawn in the Master Plan with a conceptual street section showing how the various modes could interact with one another down a singular corridor and a note that the planning and final decisions about how to reposition the railway corridor would be determined in the

coming years, recognizing that neither land ownership nor funding was (or currently is) in place for this detailed planning.

a. Adjustment to the information within the Shared Transportation Corridor description.

- i. The existing Master Plan showed a conceptual connection of the CP Rail corridor east of the site into Cretin Ave and showed that connection occurring through a singular corridor. With the recent exploration of the UST project on and adjacent to the CP Rail site, discussions have occurred as to how the various modes of transportation could be preserved to maintain the connection to the CP Rail corridor east of Cleveland in the future while also allowing for the development of the CP Rail site into a productive use now. Without a productive use for the CP Rail site (as allowed for by the UST development) there is no opportunity for the realization of this potential transit connection. There are various ways the transportation corridor could connect to and through the site and this amendment will look to provide the critical connection points while allowing the flexibility of the CP Rail site development to design a connection that would also work with the intended use of the site at a time in the future when land control of the railway corridor and funding exists for implementation.
- ii. The UST project design will preserve the various opportunities for future transportation connections to the east CP Rail corridor. There will be pedestrian and bicycle connection points weaved through the UST site. Bus transit could connect through the vehicular roadway utilized for ballfields traffic, connecting to Montreal Ave which is already contemplated for bus traffic. Rail transit could connect through the site via mixed and/or dedicated traffic conditions. For all of these transit scenarios there is no current funding plan by the local government agencies to purchase the CP Rail corridor east of Cleveland Ave, nor to implement any broader transit connections through the proposed UST ballfields site and to the east of Cleveland Avenue.

5. Open Space System

The Master Plan includes information on where the various open space elements, such as public parks, privately owned public spaces, and stormwater infrastructure are located within the development along with descriptions of what those spaces could potentially include within them.

a. Adjustment to the Open Space System for the proposed relocation of the Community Green Space and the addition of the CP Rail Parkland Dedication.

- i. The UST project is proposing to relocate the majority of the Community Green Space to the east of its current location so that it lies southeast of the Montreal Ave and Finn St intersection and in order that a street extension to connect the CP Rail parcel to the Highland Bridge development may be made. The Finn St extension will provide important activation and access to the CP Rail parcels while maintaining the street pattern to the north and providing greater access for the parkland. The

park relocation will require approval through the Parkland Diversion process, which is intended to progress simultaneously with the Master Plan Amendment process. As currently proposed the diversion would provide for greater useability of the parkland than that of the current Park D parcel.

- ii. The UST project will develop the CP Rail site at the southern end of the Highland Bridge development which will require new parkland dedication, as this parcel was not previously included with the original Ryan property purchase back in 2019. The UST project is proposing a public park space to be combined with the relocated Community Green Space (Park D) as mentioned above. This combined proposal creates a more useable Park D and will increase the overall parkland dedicated within the Highland Bridge area. The Parkland Dedication process will progress simultaneously with the Master Plan Amendment process and is required for the UST project to progress. That process will determine the method and amount of parkland dedication, both of which must comply with the requirements of the Zoning Code.



Ford Site Zoning and Public Realm Master Plan - Ryan Companies Amendments List 01.25.2023					
Change	Requested Change to Ford Zoning and Public Realm Master Plan	Page #	Corresponding Change to Zoning Code Text or Map	Section #	Justification for Request *Refer to Executive Summary Document*
1	Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses	31, 39	Exclusion added to Table 66.931	66.931	Refer to #1 FAR section
2	Modification to Table 4.7 for Civic & Institutional uses as a land use category and setting the maximum spaces allowed as provided in Section 63.207 of the Zoning Code	58	Modification to Table 66.942	66.942	Refer to #2 Parking section
3	Exclusion to the limitation of surface parking spaces within the southeasterly F6 district for Civic & Institutional uses	58	-	-	Refer to #2 Parking section
4	Exclusion to the maximum Public Right of Way Setback within the F6 district for Civic & Institutional uses	93	Exclusion added to Table 66.931	66.931	Refer to #3 Building Type Standards section
5	Adjustment to the information within the Shared Transportation Corridor description	125	-	-	Refer to #4 Shared Transportation Corridor section
6	Adjustments to the Open Space System for the proposed relocation of Community Green Space and the addition of the CP Rail Parkland Dedication	132	-	-	Refer to #5 Open Space System section



MEMO

TO: Spencer Miller-Johnson, City of St. Paul
DATE: January 25, 2023
FROM: Maureen Michalski, Project Paul c/o Ryan Companies
SUBJECT: UST Ballfields Supplemental Memo
CC: Anthony Adams, Ryan Companies
Jim Brummer, University of St. Thomas

Spencer,

Please find enclosed information to include with the UST Ballfields Master Plan Amendment application for more information on the proposed project. Supplemental imagery is provided via separate document and referenced in the text below.

Project Overview – The UST Ballfields project includes a 1,500 seat baseball field, a 1,000 seat softball field, an indoor practice facility, and associated parking for both event and non-event uses. The project is proposed at the southern end of Highland Bridge south of Montreal Ave, west of Cleveland Ave, north of Hampshire Ave, and east of Unčí Makhá Park. The current land parcels the project covers includes Block 33, Block 34, and Míča Park of the FORD plat as well as two vacant CP Rail parcels. Block 33, Block 34, and Míča Park uses are all intended to remain with some reconfiguration of parcel lines.

Community Benefits – The UST Ballfields project provides the following community benefits to the Highland Park and broader Saint Paul area:

- **Redevelopment of the CP Rail Site** – the project provides a market identified use, allowing for cleanup of the site and the creation of quality facilities and investment which will otherwise remain vacant with meaningful barriers to development such as restrictive airport overlays, site visibility and access.
- **Community Facility Use** – the project creates quality facilities that could be used for youth and community ballfield use, entertainment, and parking.
- **Enhanced Parkland** – the project improves the usable area for existing public parkland and increases the overall quantity of public parkland per the CP Rail parkland dedication.
- **Multimodal Transportation** – the project unlocks the potential for a future Ford Spur connection, enhancing pedestrian and bike trail opportunities along with the possibility of future multimodal transit corridor to the east.
- **Improved Water Quality and Wetlands** – the project improves stormwater quality and management on a currently unmanaged site, decreasing flow rate to Hidden Falls Park and improving area wetland conditions.
- **Sustainability** – the project will be built to LEED silver per UST standards.



Community Use – Members of the community will benefit from access to ballfield use, entertainment, gathering spaces, pedestrian and bicycle trail connections, new public open space, and the parking proposed as part of the project. The facilities are not envisioned to be for the sole use of the University. UST is in discussions with organizations and community sports teams for use of the ballfields beyond the use by UST and anticipate additional discussions in this regard for community use. NCAA regulations prohibit preference for any individual or groups of athletes that may be potential recruits, which will be upheld through a fee model for use to ensure equity and compliance. Beyond field time needs in the community, there is also a need for community parking as users and spectators of the Highland Ballfields presently park on the neighborhood streets due to the limited parking at their facility on the northwest corner of Cleveland and Montreal.

Design – The design of the UST Ballfields is founded on providing a memorable fan experience during events, a cohesive visitor experience both during events and during normal day to day operations of the facilities, connections to notable destinations surrounding the site area for community interaction, and placemaking design elements in order to provide high quality facilities while striving to meet the aesthetic goals of the master plan and the other various constraints of the site such as grade change and wetlands. Images have been included with this memo to show precedent images of what the facilities may be (starting on page 5 of the supplemental images) as well as the latest images of the site layout (starting on page 11 of the supplemental images). Note that these designs will progress as the project is advanced and are subject to change.

Notable design features include:

- **Baseball/Softball** – The baseball and softball facilities were sited on the western portion of the site to be setback from the main vehicular roadways and pedestrian facilities for safety purposes, to provide its own distinct entrance to the combined facilities, and to be able to orient the fields for sun angles and player safety. Access to the seating elements will be through a shared concourse either at-grade or raised. The baseball and softball facility will include an attached practice facility as well.
- **Public Spaces** – The public spaces, which includes the relocated Míča Park and the newly dedicated public park space for development of the CP Rail parcel, were sited east of the ballfields and south of Montreal Ave for convenient access to prominent pedestrian and bicycle facilities along Montreal Ave and Finn St, for adjacencies to the existing Highland Ball use, and to promote the community use of both the dedicated public park spaces but also the rest of the UST Ballfields. The current UST campus to the north is open to the public through its various walking paths and lawn spaces and the grounds surrounding the UST Ballfields are intended to be as well, so siting the dedicated public park spaces at the north end of the site was key to inviting the community into the site to use the spaces. Note that the fields themselves will be secured during non-event periods for protection of the fields and equipment and to maintain quality for community and UST athlete users.
- **Connections to Notable Destinations** – Some of the notable destinations that the site design is proposed to connect with includes the pedestrian and bicycle facilities at the southern end of Finn St, the Cretin Ave termination point, the Central Water Feature



pathways, and the existing tunnel connection to the CP Rail corridor east of Cleveland Ave. Bicycle and pedestrian connections to Finn St, the Central Water Feature, and the Cleveland Ave tunnel will provide for continuous access to and through the UST Ballfields site. A pedestrian element, whether that is simply an open plaza or some type of inviting gateway/entry feature, is anticipated at the termination point of Cretin Ave to provide a pedestrian friendly entrance to the site.

- Parking – The parking was sited interior to the site, set back from the major roadways, to push the public spaces and main facilities to the exterior for increased visibility. The parking, whether it is surface parking or future structured parking, will be designed with high quality materials and aesthetics, and the facilities that allow for more unique design features will be showcased at the site exterior.

The heights and densities for the Gateway District outlined in the master plan are in the middle of the range of the overall Ford Site zoning districts. It is anticipated that the buildings proposed for the UST Ballfields will be designed within the height range specified in the zoning district of 30-65 feet.

Event Attendance/Traffic – The capacities planned for the UST Ballfields include a maximum of 1,500 seats for baseball and 1,000 seats for softball. Unlike many other collegiate ballfields, it is anticipated that the baseball and softball facilities planned as part of the project will share service areas for restrooms, concessions, ticketing, etc. The most similarly sized local baseball stadium is Siebert Field (University of Minnesota) with ~1,500 seats. In contrast, CHS field in downtown Saint Paul (St. Paul Saints) has a capacity of ~7,200 seats which is much larger than the fields proposed. The most similarly sized local softball facility is Jane Sage Cowles Stadium (University of Minnesota) with ~1,000 seats; however, that venue has additional ancillary facilities that in the case of the UST Ballfields would be shared with baseball.

Regular practice for the UST baseball and softball teams begins September through October with two to three home games for both baseball and softball. Spring season will start when the snow melts, with home games beginning the last week of March and ending in May for regular season play. Both baseball and softball will play up to 20 home games in the spring season. Baseball games are primarily on Fridays, Saturdays and Sundays (with several Tuesday or Wednesday evening games), and softball games are primarily on Saturday and Sundays (with several Tuesday or Wednesday evening games). Post-season games may extend into early June and community, high school and youth sports game times will be variable.

Attendance of baseball and softball games on campus have not previously been tracked as the attendance has been low enough not to warrant ticketing for the events. UST's best approximation for historic on campus games is 300 people for baseball events and 200 people for softball events. Those numbers reflect a Division I program that recently transitioned from Division III and is still building to Division I attendance levels. It can reasonably be assumed that attendance will increase after the Ballfields are constructed, the fan experience improves, and the Division I program grows in popularity over time. Aspirations for higher attendance events including conference championships and regional tournaments, which will warrant a higher attendance level, should be taken into consideration as well.



Parking – The UST Ballfields project is proposing a surface parking lot to be constructed with the initial build of the facilities for the parking to grow with the increased attendance of the facilities, provide flexibility for future uses such as the shared transit corridor as well as shared use with Block 34, and to limit views for parking from the neighbors to the south of the site. A structured parking ramp would be constructed at a future date only if the surface parking lot size no longer met the demand of the ballfield attendance.

The number of surface parking stalls shown on the UST Ballfields site plan, approximately 330 stalls, reflects a 3:1 people to car parking ratio (industry standard) for the largest softball event (1,000 seat capacity) and a 3:1 ratio for roughly 2/3 of the largest baseball event (1,500 capacity). The surface parking lot also reflects the amount of parking stalls that can reasonably fit within the site area acknowledging the existing land constraints, such as existing utilities and elevation changes, as well as the other programmatic elements of the UST project, such as access to the indoor practice facility loading area and turn arounds for shuttles. Note that the surface parking lot will be subject to standards of design within the zoning code such as parking space dimensions, landscaping, lighting, and stormwater runoff.

The surface parking area is intended for community uses and not solely UST use. UST would likely sign the entry drive towards the parking lot and within the parking lot itself for time periods that the public could use the surface parking lot and times when it is reserved for University events. Public use for accessing the adjacent park, walking to the Highland Ballfields located to the north, or accessing the various multi-use trails within Highland Bridge would be available.

Comparison parking lots in St. Paul for size and scale reference include:

- Minnesota History Center Southeast Parking Lot: ~320 spaces
- Two Rivers High School North Parking Lot: ~330 spaces
- Highland Park High School: ~280 spaces
- St. Catherine University West Parking Lot: ~350 spaces

Local, comparable quality parking lots reflective of landscaping and islands are shown on page 9 of the supplemental imagery and include:

- Top Left: Minnesota History Center (St. Paul, MN)
- Right: Ecolab Research & Development (Eagan, MN)
- Bottom Left: Maple Grove Hospital (Maple Grove, MN)

Block 33 Affordable Housing – The application for Master Plan Amendments for the University of St Thomas (UST) Ballfields at the southern end of Highland Bridge and associated redevelopment agreement modifications will not include any change in the overall quantity of affordable units per the 20% of total of housing units requirement planned at Highland Bridge, nor the affordable housing area median income targets, nor the mix of affordable housing types (supportive/family/etc.). The project team plans to maintain the 110 units of affordable housing originally planned for Block 33 within a slightly modified parcel area, removing the small, unusable triangle at the southern end of the parcel and replacing that exact land area along the west side of the parcel in a more useable location, maintaining the overall size of the original



Block 33 for development. A conceptual program layout of the affordable housing on Block 33 is shown on page 12 of the supplemental images.

Block 34 Ryan Development – The application for Master Plan Amendments for the UST Ballfields at the southern end of Highland Bridge and associated redevelopment agreement modifications will not include any change in the overall quantity of development previously proposed on Block 34. The project team is planning to reduce the parcel area to accommodate the relocated and increased parkland for the UST Ballfields project, while maintaining the area to accommodate 100,000 SF of office as previously proposed within the block and outlined in the minimum assessment agreement. A conceptual layout of a potential office building (north building) and structured parking ramp (south building) within Block 34 is shown on page 12 of the supplemental images. Note that due to changes in the market conditions since the original 2019 Ryan Development plan an office building in this portion of the development may be unlikely; however, the size and shape of block 34 as proposed to be re-platted will allow for a variety of potential uses to activate this corner and to meet the minimum assessment requirement. The project team is also exploring ways to share Block 34 parking with the UST parking area which may remove or reduce the structured parking ramp within Block 34.

Jobs – Ryan estimated approximately 1,300 full time jobs and 14,500 construction jobs for the full build out of Highland Bridge based on the Ryan Development Plan in 2019. These did not include any assumptions for the CP Rail parcels at the southern end of Highland Bridge. Ryan is modifying both the Block 33 and Block 34 parcel areas but is maintaining the affordable housing and office development components previously anticipated, so there are no anticipated changes to the projected jobs. The UST Ballfields will bring its own number of jobs to the development to add to the 2019 Ryan estimates above, including daily employees stationed at the fields as well as seasonal employees during the baseball and softball seasons. This is estimated to be 20 seasonal employees and 5 daily employees.

Taxes and Other Revenue – CP Rail previously paid very minimal taxes at the county level, specifically for special assessments (\$5,500 in 2022), related to their parcels that are part of the planned UST development. The Minnesota Department of Revenue (MNDOR) receives taxes from the railroads based on miles of main line track in the railroad's network throughout the state. The MNDOR then distributes to any county their proportionate share of this tax based on the amount of main line track in that county. The track located on the CP Rail site in consideration is not part of MNDOR's calculation of tax revenue collection from CP Rail presently, which is why it has been able to be removed. Therefore, the land that CP Rail previously owned, now owned by the Capital City Properties (CCP), at Highland Bridge is not a meaningful contributor to the property tax base in St. Paul as the only revenue through tax payments paid by on the land in recent years is for the limited special assessments amount noted above.

The Block 34 parcel currently owned by Ryan within the Highland Bridge redevelopment will pay taxes per the tax-increment financing (TIF) district minimum assessment value outlined for the



original platted Block 34 area in the Minimum Assessment Agreement between Ryan and the City, thus retaining the same level of property taxes as in place now and planned. The Ryan and UST team are working with the City Office of Financial Services and Ramsey County to memorialize updated legal descriptions for the to be re-platted parcels in an amendment to the redevelopment agreement. An amendment will be needed to accurately describe the modifications to the physical area within the TIF district and the re-platted areas.

In terms of other revenue sources, UST will pay sales tax on ticket revenue, fan merchandise, and any concessions that may be sold on site. There will also be fees paid to the city for permitting and value of land associated with parkland donation that the city would not be a beneficiary of without an identified use for the CP Rail site.

Goals and Vision of Master Plan – The proposed UST Ballfields at Highland Bridge are compatible with the goals and vision of the master plan to create a “vibrant, mixed-use, urban neighborhood with interwoven green space.” **The master plan identifies civic and institutional uses as a requested use type for the gateway zoning district and requires a minimum of 50,000 square feet within the entirety of Highland Bridge.** At the time of its drafting, the master plan could not have contemplated all possible ways in which a civic and institutional use might present itself, which is why there is language in the master plan which notes that it is “anticipated that this document may be updated over the course of the site’s final planning and expected decade plus build out, in order to accurately reflect evolving conditions, opportunities and priorities for site redevelopment.”

There have been many meaningful events that have taken place both locally and in the world that have impacted the development of Highland Bridge over the past few years such as the Covid-19 pandemic, Russia’s war with Ukraine, and locally the adoption of a rent control policy to name a few. Impacts that have come out of these meaningful events include a significant increase in inflation and interest rates, significant cost and delay in sourcing construction materials, a shift in operations for many businesses such as remote or hybrid workplaces, and reluctance from investors to support certain development product types. The UST Ballfields is an opportunity to bring a user to the development who will retain ownership of the land long term (UST has owned and operated their current campus for over 100 years), has confidence in developing this land with all of the volatile market conditions in the world right now, will provide a use that is a direct benefit to the community, and will provide a high quality of design and maintenance of their facilities improving the current condition of the site. Changes need to be allowed to the master plan document in order to react to these ever-changing market conditions so the Highland Bridge development can be put to a productive use for the community and the City.

The UST Ballfields will support the following guiding principles of the Master Plan:

- **Mix of Uses and Activities** – The proposed use “complements and integrates” into the neighborhood in the southeast segment of the site to a greater degree than a proposed office building or industrial facility might, with increased public spaces, transit connections and field areas. This proposed civic/institutional use is identified in the master plan as a use encouraged and allowed within the development and specifically



the Gateway zoning district. This use also enhances and expands upon recreation amenities in the Highland Bridge development and the surrounding neighborhood. Most importantly, it provides a use for the CP Rail site which will significantly increase the vibrancy at the south end of the site that will not happen without an identified use for this parcel.

- **Jobs and Tax Base** – The jobs and tax base are increased through “Increased regional significance and economic value” and a “Range of business and employment opportunities”. Events at the facilities will create a destination to pull people into the area for support of local retail and restaurants, generating significant spending annually for St. Paul businesses.
- **Energy and Sustainability** – All buildings will be LEED Certified and comply with SB2030 and the St. Paul Sustainable Building Overlay.
- **Transportation Choice** – The multimodal connection opportunities are expanded now and in the future through the preservation of a transit corridor and extensions of current bicycle and pedestrian facilities. Further, the use supports a “mix and density of activities to support transit”. This additional use allows greater opportunities to support transit at this site than a lower use, lower density option that may eventually be developed here due to market conditions.
- **Parks and Amenities** – The parks and amenities are enhanced by including “active places for people to play” with new indoor and outdoor facilities that will have availability for use by youth and other community sports teams.

Multimodal Transit – The design of the proposed UST Ballfields includes a variety of connections to expand the existing bicycle, pedestrian and transit networks through the site as well as accommodate the land for possible further extensions of these networks in the future. Different types of transit opportunities are being considered in the design including possibilities for future bus or rail. Opportunities for future transit through the site will be memorialized through easements over privately owned land as part of the site plan review process for the project. This will ensure that should transit, whether that be bus or rail, occur in the area in the future, it will be possible to connect north from Cretin Avenue and Montreal Avenue (as the new Cretin Avenue is designed for) and east to the Canadian Pacific Railway land that extends from Cleveland Avenue to downtown Saint Paul.

FAA/MAC Overlay – A federal and local flight overlay zone limits the ability to provide buildings with occupiable uses within the UST Ballfields project. Page 13 of the supplemental images shows this area extending over the westerly side of the Block 34 parcel as well as significant portions of the CP Rail parcel. These images show the challenges with development of the CP Rail parcel for other uses, compounding the other challenges of the site- visibility due to grade changes and site access.



Parallel Processes – There are multiple approval processes required for advancement of the UST Ballfields proposal. The key approvals and approximate timing are indicated below.

City Approvals

- AUAR Update – Start January 2023, Complete June 2023
- Master Plan Amendments – Start January 2023, Complete July/August 2023
- Parkland Swap and Dedication – Start January 2023, Complete July/August 2023
- Redevelopment Agreement Modifications – Start January 2023, Complete July/August 2023
- Plat – Start March 2023, Complete July/August 2023

Other Approvals

- Airport Overlay Approvals – Start March 2023, Complete May 2023
- Wetland Approvals – Start January 2023, Complete May 2023



Supplemental Images

Highland Bridge: UST Ballfields



Project Location

Highland Bridge: UST Ballfields

UST Ballfields – Location



**Current Development Proposal:
UST Ballfields**

UST Ballfields – Location

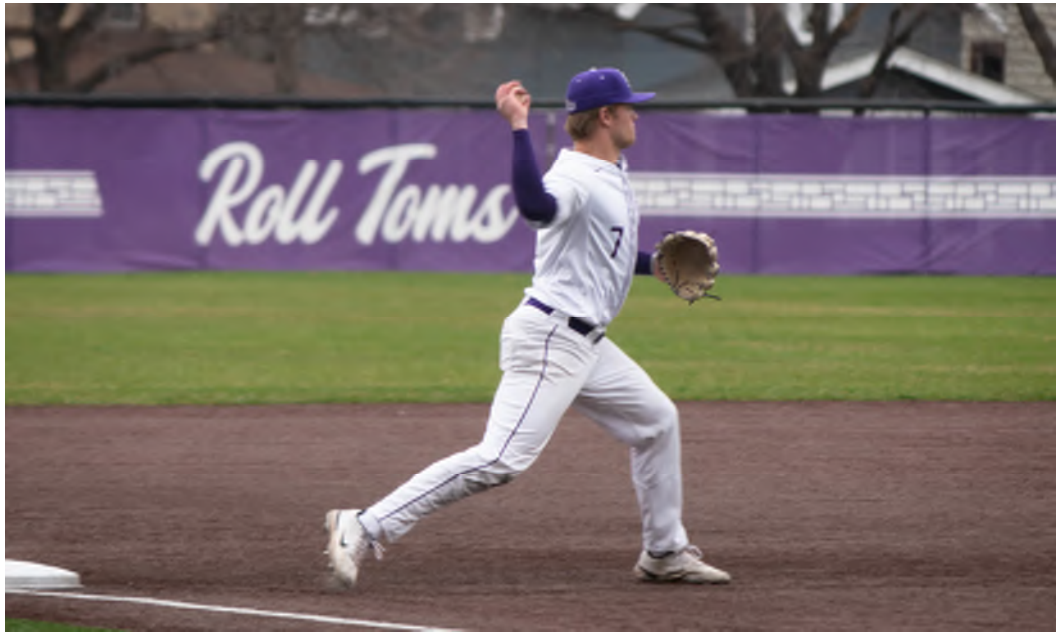




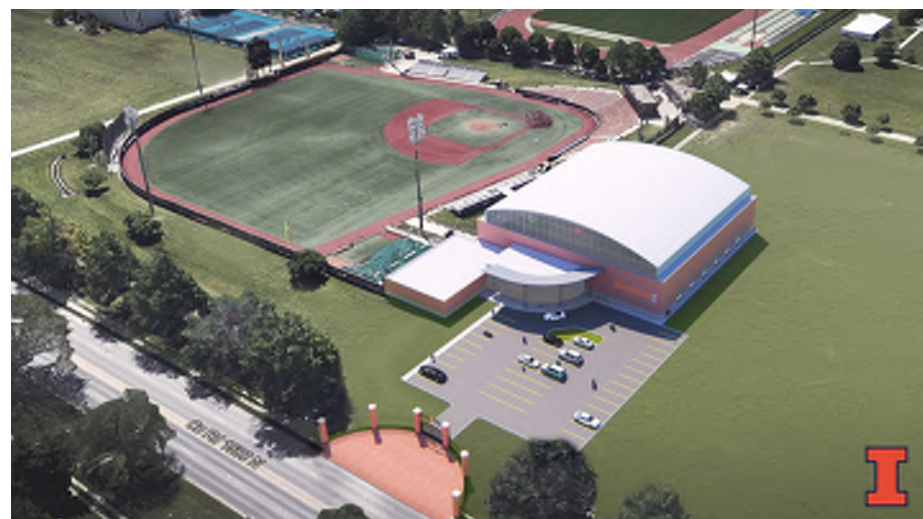
Precedent Imagery

Highland Bridge: UST Ballfields





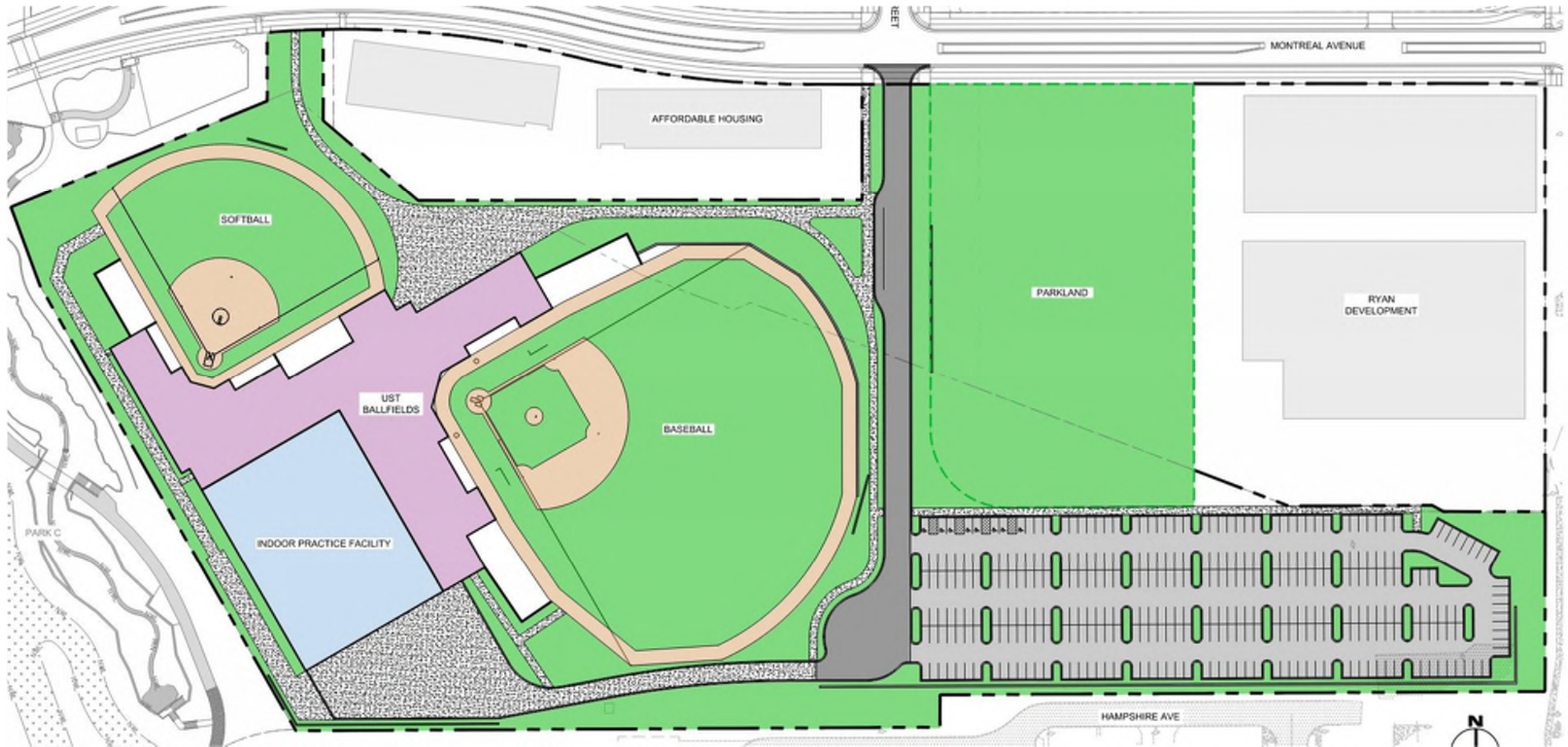


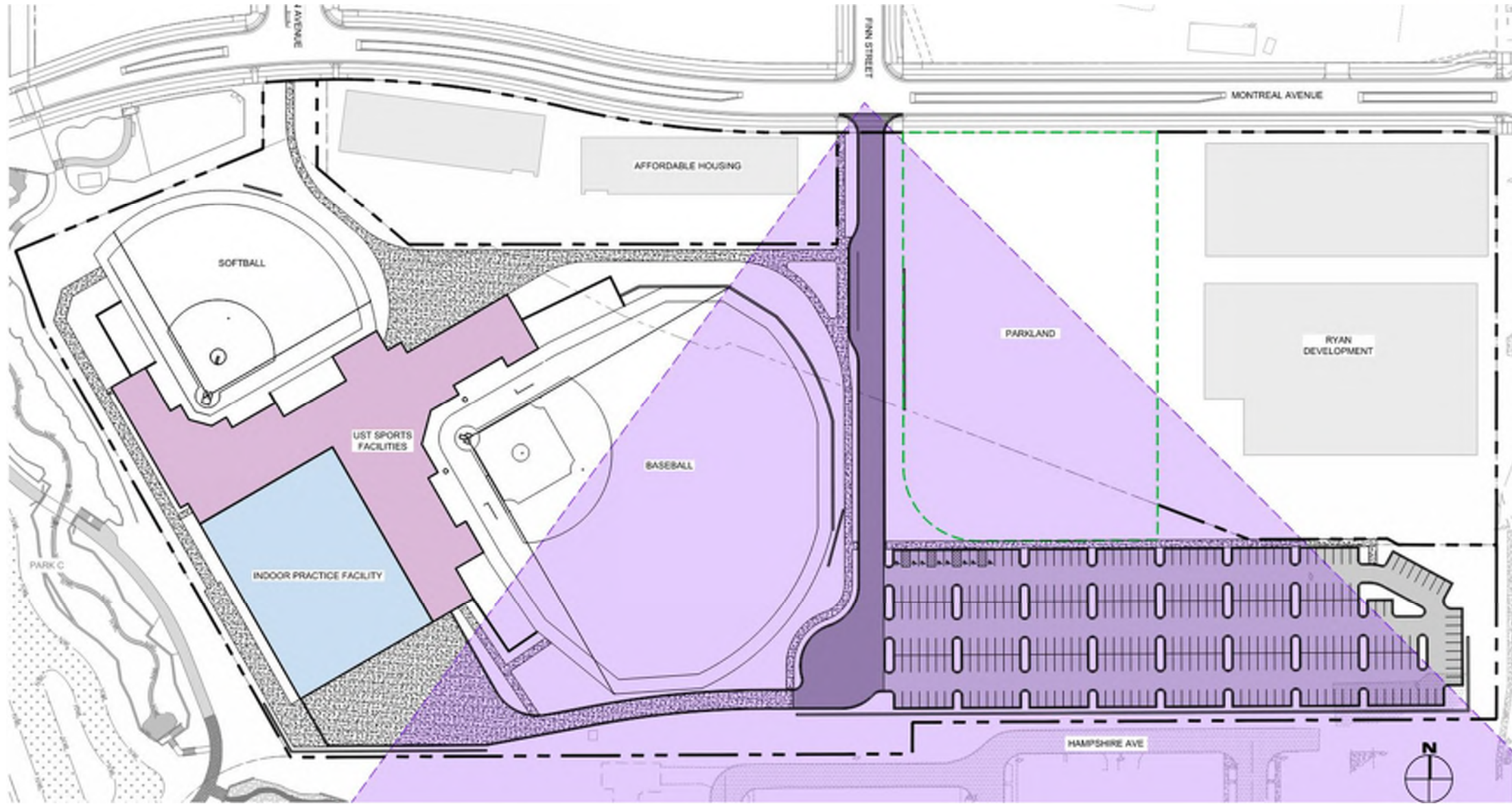




Project Images

Highland Bridge: UST Ballfields





Occupiable uses in Airport Overlay zone require a MAC variance

Rendered Site Plan

Attachment 3 - Ryan Companies Application and Materials



Rendered Site Plan



Greyed out buildings to the north and east of the ballfields are not a part of the UST project

View From South End of Unčí Makhá Park (Looking Northeast)

Attachment 3 Ryan Companies Application and Materials



View From South End of Central Water Feature (Looking Southeast)



Building shown on the left in this image is not a part of the UST project

View From Montreal Ave/Finn St Intersection (Looking South)



Building shown on the right in this image is not a part of the UST project

View From Cleveland Ave Bridge (Looking West)

Attachment 3 - Ryan Companies Application and Materials



View From Hampshire Ave – Landscape Screening (Looking North)

Attachment 3 - Ryan Companies Application and Materials



Building shown within this image is not a part of the UST project

View From Hampshire Ave – No Landscape Screening (Looking North)

Attachment 2 - Ryan Companies Application and Materials



Building shown within this image is not a part of the UST project



MEMO

TO: Spencer Miller Johnson, City of Saint Paul Planning
 DATE: April 20, 2023
 FROM: Maureen Michalski
 SUBJECT: Public Comment and Supplement to 1-25-23 Master Plan Amendment Application for UST Ballfields
 CC: Tia Andersen, City of Saint Paul DSI

Please find enclosed additional information pertinent to the UST Ballfields Master Plan application submitted by Ryan Companies. Below items clarify questions and comments arising from the 3-1-23 and 3-15-23 CNPC Meetings and the 3-17-23 PC Meeting.

Approvals

- There have been various questions and comments about overlapping City of St. Paul (City) processes. In addition to the Master Plan Amendments process, of which the Planning Commission has purview, Ryan Companies (Ryan) on behalf of the University of St. Thomas (UST) are also advancing several other processes with City staff and other commissions, over which they have purview, that will culminate in a final City Council public hearing targeted at the end of June.
 - AUAR Update: The AUAR Update is being undertaken presently by Kimley Horn in a contract with the City to address items such as traffic and environmental impacts within Highland Bridge with the addition of the UST Ballfields project. There will be a public comment period in May with regards to these details. The process must be concluded prior to any other land use approval taking place, currently targeting mid-June for completion.
 - Parkland Approvals: The parkland swap and dedication process is being led by City of St Paul Parks and Recreation Department staff and will be in front of the Parks and Recreation Commission at their May 11th meeting and at the final City Council public hearing in June. As part of the process Ryan is working with the City Attorney's office on documents memorializing the public benefits package for parkland diversion and dedication.
 - Plat: The platting process is underway with City staff and will be before the City Council in June.
 - Legal Documents: Legal documents are being drafted to memorialize various details associated with the development in partnership with the Mayor's office, City staff and the City Attorney's office.
 - Zoning Applications: Zoning applications would happen in the future as the actual design of the ballfields and associated facilities advances.
- Approvals that are not City specific include those associated with the airport overlay and wetlands.
 - Airport Overlay: The Metropolitan Airports Commission has purview over the airport overlay approvals and we have been discussing the project with staff and will submit for approvals related to the ballfields project concept officially in May.



- Wetlands: The wetlands are under purview of a Technical Evaluation Panel led by the City Water Resources Engineer with input from the Capitol Region Watershed District, Ramsey County, and the Board of Soil and Water Resources and we have been in discussions with them for many months to sort out details of a revised wetland layout for the area. We continue to refine and advance the details of wetland planning with the Technical Evaluation Panel.

Community Engagement

- **Question:** Commissioner Mitchell asked about community engagement and encouraged UST and Ryan to engage with the community.
- **Additional Information:** UST and Ryan have been engaging with the Highland District Council (HDC) Community Development Committee and the broader neighborhood since January of 2022. In late spring of 2022, the University decided to modify the program for the area to focus on baseball and softball facilities and remove a plan for hockey at this location. The engagement with the HDC over many months has resulted in a support letter from the neighborhood group which was submitted as a public comment from the Executive Director of the District Council. In addition to regular conversations with the official neighborhood group, UST has hosted two community meetings: August 2022 with ~90 attendees and February of 2023 with ~40 attendees. An email list of attendees from the first meeting received follow information and digital access (which was also publicly posted on the Highland Bridge and Highland District Council websites) to the meeting presentation with written question and answers arising from the meeting. Attendees were also informed via email of the subsequent meeting and provided with digital access to posted materials from the subsequent meeting. The supplemental attachment to this memo includes digital links to the meeting minutes from the Highland District Council meetings, and links to copies of the meeting presentations. UST and Ryan have met with individual neighbors as well and will continue to do so as design details for the proposal advance.

Public Use of Ballfields

- The UST Ballfields are entirely privately funded and owned. However, they will also be accessible to public and private groups for a fee on a scheduled basis when not in use by UST. Per Division I NCAA regulations, UST is not able to provide preferential treatment to any singular entity or person as related to potential impacts on prospective student-athletes and their eligibility. The UST use is anticipated to be approximately 50 times per year for games as well as regular practice times. Access for youth and community sports is one of the myriad of public benefits these privately funded facilities will provide. At present, UST along with the City and Ryan, are in the process of reviewing and finalizing several legal documents related to the development. These documents cover a range of items, including the accessibility of the facilities for public and private groups. The UST team will work closely with the City team to refine these documents and address any further details as the development progresses.

Public Use of Parking Lot

- **Question:** Commissioner Ortega asked if the public could use the parking and, if so, when.
- **Additional Information:** As noted in the CNPC meeting, parking will be available for community use at times when there is not scheduled use of the facilities. Community parking has been an interest of the neighborhood both to reduce the on-street parking seen in the adjacent



neighborhood from the Highland Ball youth sports fields, as well as to support the newly developed and to be developed parks within the Highland Bridge community. We have provided an attachment of relevant slides from the recent community meetings that highlights the anticipated baseball and softball schedules. The team is estimating ~50 times per year where there would be UST events that the parking lot would be restricted. There may be additional events through community and other entity uses of the fields that would require restricting public use of the parking. Security in the area is also important and parking may be restricted to reasonable hours to account for this concern.

Surface Parking vs Structured Parking

- **Question:** Commissioner Martinson asked why last year's proposal included structured parking and why this year's proposal includes surface parking. Commissioner Holst expressed concern regarding surface parking generally and inquired about what the parking structure might look like.
- **Additional Information:** The proposal submitted earlier in 2022 by Ryan Companies for the UST Sports Facilities included a 5,000 seat hockey arena (along with a second sheet of ice) in addition to baseball and softball facilities. The hockey arena is no longer proposed as a part of this development at Highland Bridge and therefore significantly less parking is required for the ballfield facilities. The surface parking is aligned with the campus design for civic and institutional uses and will comply with the off-street parking regulations of the zoning code such as landscaping, lighting, and stormwater runoff.

In terms of attendance at events and use of the parking, UST has communicated at community events that they very recently transitioned from Division III to Division I sports and only see a couple of hundred attendees for baseball/softball games on campus today due to their current facility capacities. They foresee attendance at their events growing as they become more of an established Division I program, which is the reason they are providing larger capacity concourse seating while looking to construct a smaller amount of parking stalls in the form of surface parking at this time.

Typically, the ratio of attendees to parking stalls for collegiate sporting events is approximately 3:1, equating to 330 stalls for softball (1,000 seats) at full capacity and 500 stalls for baseball (1,500 seats) at full capacity. It is rare that these teams would have events that would occur concurrently. Please see attached supplementary information packet regarding details of the softball and baseball program schedule, which equates to roughly 50 events per year.

With regards to alternate transit, UST presently offers shuttles to offsite games for their hockey program (games are played in Mendota Heights) and would anticipate offering shuttles for students as well as shuttling athletes to events at these new proposed ballfields.

The surface parking allows for flexibility on a variety of levels for the site and community. There is currently a lack of demand for structured parking based on attendance. As program attendance grows, alternative transit and transportation forms may develop in the future and make a structured parking facility irrelevant. There is also the future potential for shared parking as the adjacent Ryan Block 34 project is developed, should uses be compatible. The surface parking maintains flexibility around the future shared transportation corridor that may connect future transit opportunities to the east of Cleveland Ave and towards downtown St. Paul.



The surface parking has had positive community feedback regarding the proposed design and location over structured parking. Due to grade changes on the site, the parking is lower in elevation than the adjacent Cleveland Avenue and single family neighborhood (south of Hampshire Ave) by approximately 15 feet. This allows for views from Hampshire Ave and Cleveland Ave to be of tree canopies with the surface parking lot as opposed to building façade with a parking structure. This area is also set back over 400 feet from Montreal Avenue and as shown in the renderings, is significantly lower in elevation than Montreal Ave with landscaping and grade change screening it from the main Highland Bridge development. With landscaping, the surface parking will be much less visible to the adjacent neighbors than a structured parking deck, which could not reasonably be expected to be fully screened by landscaping. Further, the surface parking has been designed and placed specifically to maintain flexibility for the long term planning in this area as it relates to the Ford Spur and transit corridor. The desire of UST and the neighbors is to minimize the parking structure until such time as it may be necessary to accommodate the volume of attendees. The neighborhood feedback has been positive regarding having additional parking available in this area to serve the Highland Ball little league fields as well as the nearby parks. Please see attached supplementary information packet including renderings which illustrate the parking areas as viewed from Cleveland Avenue and Hillcrest Avenue to the south as well as comparisons showing impervious area and tree coverage as related to the surface vs structured parking options. Note that City code has specific landscaping requirements for surface parking lots that are not present with structured parking ramps.

Shared Transportation Corridor

- **Question:** Commissioner Martinson asked about the shared transportation connection between Cretin and Cleveland. Commissioner Rangel Morales commented regarding legal arrangements and future obligations of the project.
- **Additional Information:** Ryan and UST have been collaborating with City staff on the shared transportation corridor and points of access for various forms of transit. Although not yet finalized, we are making progress towards determining the pathways that will be utilized for various multimodal options. A set of images showing the probable locations of these pathways has been included in your package. These pathways, once finalized, will be documented through easements to the City's benefit during the site plan review process and before the issuance of building permits. The CP Rail spur to the east of Cleveland Ave is presently owned by Canadian Pacific Railway. There is presently not funding identified for the acquisition of this corridor into downtown St. Paul nor the development of any potential transit. Ramsey County is in process of a study of transit options in this area.

Design of Buildings Facing Uñčí Makhá Park

- **Question:** Commissioner Grill ask about design of the practice facility and relationship to the existing park.
- **Additional Information:** It is anticipated that the practice facility and concourse buildings facing Uñčí Makhá Park will be set back approximately 30 feet from the property line to allow for fire and maintenance access to the west sides of these buildings. Further, there is a grade change between the CP Rail parcel and Uñčí Makhá Park which will require retaining walls with fall protection fencing. It is anticipated that an approximately 8-12 foot landscape buffer will exist between fire/maintenance access and the edge of the property line facing the park along with



another 12-16 foot landscape buffer between the fire/maintenance access and the proposed buildings. Further, it is the intention of UST that the design of the west side of the facilities will be “four sided” architecture which engages with the park. Design will be further advanced after completion of Master Plan amendment approvals and project fundraising. Materials showing current site conditions, grading, and massing imagery of potential facilities are included in your package. Please note that rendered images show massing only as ballfields facilities are not yet designed. When designed we anticipated their heights will be lower than the Mississippi River Corridor Critical Area (MRCCA) height restrictions that are over a portion of the site and well within the 65’ height limit allowed in the F6 Gateway zoning district. The ballfields facilities are also much lower density than allowed for within the zoning district, as evidenced by the FAR Master Plan amendment request.

Fields in F6 District

- **Question:** Commissioner Grill asked if the previous recreational fields as identified in the staff report were meant to be public or private.
- **Additional Information:** The staff report highlights “a similar recreational use of the southeast portion of the Highland Bridge site was envisioned during Master Plan development due to interest from several private groups.” Therefore, it does appear there were private groups previously interested.

While UST is proposing these fields for the use of their baseball and softball programs, and funding them privately, they have also been clear in community meetings that they intend to offer access to community groups on a scheduled, fee-basis as well. They have begun conversation with community based youth sports groups, such as Friends of Highland Ball (supporting 1,000 youth athletes in the community), to allow for use by youth sports teams in the area. The fields would operate with scheduled use for non-UST groups and would be secured when not in use to maintain safety and security.

Sustainability and Solar

- **Question:** Commissioner Martinson asked about the inclusion of solar power.
- **Additional Information:** UST has made a commitment to build their buildings at a minimum to LEED Silver standards. Further, UST is a leader in solar research through their Center for MicroGrid Research (<https://engineering.stthomas.edu/industry/microgrid-research-center/>). They presently have solar panels on many of their on-campus buildings and will be considering the potential for solar at these facilities as design advances. The existing Master Plan also has solar guidelines and other sustainability requirements such as bike parking and EV-ready charging stalls.

Parkland Improvements & Benefit Package

- Although not the purview of the Planning Commission, a separate process is underway with regards to the parkland swap and dedication needed to advance the UST Ballfields project. UST and Ryan have been working with City Parks staff, the Mayor’s office, and the City Attorney’s office on a comprehensive public benefits package. The Parks Commission meeting with regards to this package will occur on May 11th, 2023.



- The City of St. Paul Parks and Recreation Department owns a 1.50-acre land area known as Park D, or Miča Park, which is presently unimproved. Due to existing and proposed wetlands as well as steep slopes within the parcel, only approximately 25% of the park land is currently available for use. The construction of the ballfields project will create an opportunity to relocate Miča Park to a site that is 38% more usable than its current location, and unencumbered by wetlands or steep slopes, which would not have been possible otherwise. The new park location also increases street frontage/access by 230% from the current land-locked park parcel. As part of the parkland diversion process, an independent appraiser was hired by the City to evaluate the value of the existing and future land area to ensure that there is no net loss in value in accordance with the City Charter. The appraisal process has been completed, and the results indicate that the value of the park property will be increased by \$227k.
- Additional public benefits as part of the comprehensive parkland diversion and dedication process include:
 - A cash payment of \$250,000, doubling the existing Miča Park budget of \$245,000, providing enhanced amenities for the community.
 - Public and private youth and community group access (including Parks led programs) to the privately owned and privately funded facilities (with an anticipated investment value of \$38M) on a scheduled, fee basis when not in use by UST.
 - Community parking access when not utilized by UST for their games (~50 times/year) or events scheduled by their youth/community partners. This will benefit Highland Ball (serving 1,000 youth athletes) as well as visitors to the nearby City of St. Paul public parks. The City is not contributing financially to the building of this ~\$2M parking lot, which will be paid for privately by UST.
 - Civil engineering costs for improved Miča Park valued at \$10,000.

Property taxes

- The CP Rail site presently generates low tax revenue, with assessments totaling only around \$5k per year and no other property taxes paid. Given this, the site is an ideal location for achieving the desired institutional use for the Ford Site, as desired in the Master Plan, since it will not impact the anticipated minimum assessment values for the Highland Bridge site. The new use will not result in any loss of property tax revenue. Furthermore, the development of the adjacent Ryan properties can proceed as planned according to the original redevelopment agreement, generating and paying for the minimum assessments as agreed to by Ryan Companies and the City of St Paul in the redevelopment agreement.

The UST pays sales tax on ticket sales as well as on cash food sales, bookstore sales, and more. In 2022, UST paid \$330k to the state of MN in sales and use tax. \$30k of this was specifically for Athletic tickets. While UST is exempt from property taxes as a nonprofit educational entity, it does pay assessments and fees to the City which range from \$85,000 to \$150,000 per year, depending on the activity of the city. UST also repairs many city amenities – sidewalks on city property, curb and gutter, etc. at the university's expense.

Other Uses

- Ryan Companies did not acquire the CP Rail parcel alongside the Ford Site in 2019 as there were no viable uses for the site at that time along with the fact that the site has a variety of physical and regulatory challenges. The various challenges make development for alternate uses difficult. A federal and local flight overlay zone limits the ability to provide buildings with occupiable uses (as seen in the supplemental information provided). The westerly portion of the



site that is not subject to the airport overlay is restricted by the Mississippi River Critical Corridor Area. Further compounding development challenges include substantial grade changes on the site, visibility from major roadways and site access.

Wetlands

- Ryan Companies has worked in partnership with the City Water Resources Coordinator and the Capitol Region Watershed district to manage the current wetlands on the site as well as the relocation of wetlands that will be required as part of the public right of way work for the Finn Street/Montreal Avenue area to the north of the proposed UST facilities. The only wetlands on the CP Rail parcel have been determined by the City, Watershed District, and State to be incidental, meaning they are man-made and were created in order to facilitate drainage for the prior rail lines on site. Therefore, these wetlands do not need to be preserved. There are existing wetlands on the parcel currently owned by the City of St Paul Parks department that also span across the site boundary to Ryan's adjacent Block 34 property. This wetland will largely remain in place as part of the redevelopment project and is labeled in the attached supplemental information as Wetland B. As part of the planning for the UST Ballfields project the relocated wetlands that were previously planned for the Park D site will now be moved to allow for the Finn Street extension into the UST project area. These wetlands will be located to the south of the existing Wetland B as indicated by the attached supplement. Wetland management and siting is a requirement of the site plan review and approval by the Capitol Region Watershed District and City Team.



MEMO

TO: Spencer Miller Johnson, City of Saint Paul Planning
 DATE: May 1, 2023
 FROM: Maureen Michalski, Ryan Companies
 SUBJECT: Public Comment and Supplement to 1-25-23 Master Plan Amendment Application for UST Ballfields
 CC: Tia Andersen, City of Saint Paul DSI

Please find enclosed additional information pertinent to the University of St. Thomas (UST) Ballfields Master Plan application submitted by Ryan Companies (Ryan). Below items clarify questions and comments arising from the 4-28-23 Planning Commission Public Hearing Meeting.

Public Use of Parking Lot and Ballfields

- Questions were raised regarding public access to the UST Ballfields and surface parking.
 - The UST Ballfields and surface parking will be entirely privately funded and owned by the university in order to support the university's Division I athletic programs. Access to the ballfields and parking by other groups is an additional benefit of this development. The ballfields will be available for scheduled, fee-based use by both public and private groups when not in use by the university. The surface parking will be available to the public when not in use by the university or groups scheduled at the ballfield facilities.
 - Ryan and UST have been actively working on a legal document that will memorialize UST's stated commitment to allow access to both the ballfields and the surface parking. This specific legal document is being prepared in partnership with the City of St. Paul (City) Attorney's Office, Mayor's Office, and Parks and Recreation Department. We anticipate this document, and other legal agreements, will be in final form before City Council action on Master Plan Amendments and Plat anticipated in June 2023.
 - Per Division I NCAA regulations, UST is not able to provide preferential treatment to any singular entity or person as related to potential impacts on prospective student-athletes and their eligibility.

Memorializing Community Benefits

- Questions were raised regarding how the various community benefits would be memorialized with the City.
 - As noted above, there will be a legal document to memorialize UST's stated commitment to allow access to ballfields and surface parking when not utilized for their primary purpose by UST, as well as to address other logistics as desired by the City and project team. This agreement has been drafted and is being finalized in partnership with the City Attorney's Office, Mayor's Office, and Parks & Recreation Department.
 - As stated in the meeting, and in prior submittals, the university will be providing easements within their site to the public/city benefit for future ped/bike and transit opportunities as part of the site plan review process. The project team has worked with City staff on these potential routes through or adjacent to the UST site to meet City and



- other regulatory design requirements so those opportunities can successfully be designed in the future if funding becomes available for those projects.
- The public parkland benefits associated with the Míča Park relocation and CP Rail parcel parkland dedication, including increased useable parkland (by 38%), increased land value of \$227k, increased park frontage along Montreal Ave, and \$250k in cash and \$10k in design work that will double the existing Míča Park budget, will be memorialized via City Council resolutions and parks CIB committee action. The land dedication of the new public parkland area will be memorialized with the recording of the plat later this summer and subsequent Deed filing.

Fee for Current UST Facility Use and List of Users

- Questions were raised regarding the fees for the use of current UST facilities and the need for transparency regarding who from the community is using these facilities.
 - The fees for the proposed ballfields have not been set at this time. Facility fees will not be set until there is a more defined timeline for completion as well as understanding of project design and amenities. Fees will be established related to the marketplace and account for facility and equipment depreciation, insurance, staffing, custodial, utilities, and other expenses.

Examples of groups that have rented space on campus in the past year at UST include:

- | | |
|-----------------------------------|--|
| Saint Paul's Outreach | MN Special Olympics |
| Ramsey Middle School | MN Aviation Career Education |
| Highland Catholic School | St. Paul Conservatory of Music Institute |
| Interfaith Action | CFG Retirement Group |
| MN Math & Science | Schulze Family Foundation |
| MN Music Coalition | MN Boy Choir |
| MN Aviation Career Education | YWCA |
| Lincoln International High School | CommonBond Communities |
| Venture Academy | Horace Mann School |
| Homegrown Lacrosse MN | MN Chess Association |
| Camp Masala | Cretin-Durham Hall |

Current Baseball and Softball rental fees for UST (Division 3):

	Full day	Hourly
Koch Diamond	\$800	\$125
Softball Field	\$750	\$125

*Subject to change

The current UST facility amenities are not comparable to proposed Division 1 facilities. New facility fees will be set to respond to the marketplace as well as account for facility and equipment depreciation, insurance, staffing for scheduling, utilities, and other expenses.



Limited Access to Facilities/Scheduling

- Comments were made regarding open facility access and the fee-based, scheduled use of the proposed UST Ballfields.
 - It is common practice for higher education to require a fee (rent and expenses), liability insurance and scheduling for use of their sports facilities. For example, the University of Minnesota also provides fee-based facility rental opportunities for their sports facilities. None of the University of Minnesota facilities allow for free public access.
 - It is highly uncommon for Division I sports facilities to be available for public access in any form (even on a scheduled, fee basis)
 - Publicly-owned facilities, such as some within the City of Saint Paul, also may require a fee for use and scheduling. For example, Jimmy Lee Recreation Center has a multi-use field that is only allowed to be used for a fee or as part of an organized recreational team of the facility. Outside of community hours, which are currently held on Saturdays from 2-4 p.m., there is limited public access to the field, depending on the rental schedule. The cost to rent the field is \$110 per hour. Similarly, Dunning fields require a fee for use and have limited access for public use. These fees are used to supplement the cost to maintain these amenities for the community.
 - It was incorrectly stated by Ryan Companies in the meeting that the Highland Ball Fields are open access. Rather, Friends of Highland Ball secures their fields when not in use.

Facilities Details and Timing

- Questions were asked about the specificity of various items and why they are not yet finalized.
 - The master plan amendments are the first step in the process for UST to understand if a ballfields complex is viable at the proposed location. Once processes are completed this summer regarding master plan amendments, AUAR update, platting, parkland, wetlands and airport approvals, the university will continue private fundraising efforts that are necessary in order to build these facilities. When sufficient fundraising has occurred, detailed design will advance, as will site plan review and facilities operation planning.

Property Taxes and Revenue to City/State

- Questions were raised regarding UST'S tax status and payments to local units of government outside of property taxes.
 - The CP Rail site presently generates very low tax revenue, with assessments totaling only around \$5k per year and no other property taxes paid. Given this, the site is an ideal location for achieving the desired institutional use for the Ford Site, as required in the Master Plan, since it will not impact the anticipated minimum assessment values for the Highland Bridge site. The new use will not result in any loss of property tax revenue from current conditions. Furthermore, the development of the adjacent Ryan properties could proceed as planned according to the original redevelopment agreement. The adjacent Ryan properties will continue to pay the minimum assessments as agreed to by Ryan and the City in the redevelopment agreement.
 - UST pays sales tax on ticket sales as well as on cash food sales, bookstore sales, and more. In 2022, UST paid \$330k to the state of MN in sales and use tax. \$30k of this was specifically for Athletic tickets. While UST is exempt from property taxes as a nonprofit educational entity, it does pay assessments and fees to the City which range from



\$85,000 to \$150,000 per year, depending on the activity of the city. UST also repairs many city amenities – sidewalks on city property, curb and gutter, etc. at the university's expense.

Neighbor Support Letters for Surface Parking

- Comments were made that Ryan and the University of St Thomas perhaps did not share all site plan renderings with neighbors and therefore the letters of support received may be based on mis-represented information.
 - Please see page four of the Highland District Council Meeting minutes from the March 21, 2023 meeting showing the rendering referenced at Planning Commission. It is publicly posted here:
[Highland District Council Community Development Committee March 21, 2023](https://www.highlanddistrictcouncil.org/wp-content/uploads/2023/03/CDC-Committee-Minutes-03212023-2.pdf)
 URL: <https://www.highlanddistrictcouncil.org/wp-content/uploads/2023/03/CDC-Committee-Minutes-03212023-2.pdf>
 - Please see page eleven of the presentation from the broad community meeting of February 28, 2023 that was held at Gloria Dei Church (700 Snelling Ave) and subsequently posted to both the Highland Bridge website and Highland District Council website. Those public postings are here:
[Community Meeting February 28, 2023 at Gloria Dei](https://highlandbridge.com/wp-content/uploads/sites/9/2023/03/UST-Ballfields-Community-Meeting-02.28.2023.pdf)
 URL: <https://highlandbridge.com/wp-content/uploads/sites/9/2023/03/UST-Ballfields-Community-Meeting-02.28.2023.pdf>
[UST/Ryan Community Meeting February 28 at Gloria Dei](https://www.highlanddistrictcouncil.org/wp-content/uploads/2023/03/UST-Ballfields-Community-Meeting-02.28.2023.pdf)
 URL: <https://www.highlanddistrictcouncil.org/wp-content/uploads/2023/03/UST-Ballfields-Community-Meeting-02.28.2023.pdf>
 - The initial public comment submittal by Ryan electronically (for the 4-28-23 meeting) also includes hyperlinks to all of the meetings which have public postings of minutes or content.

Other Uses

- Questions were raised on any impacts to affordable housing units or timing because of the UST project.
 - As stated in the meeting, the redevelopment agreement between Ryan and the City determined the location and quantity of affordable units throughout the Highland Bridge Development and the UST project will not change that plan. The affordable housing projects, which will be developed by Project for Pride in Living and CommonBond Communities, are dependent on when they can successfully receive enough funding to advance. The redevelopment agreement is included in the link below with Exhibit EE showing the location, percentage of AMI, number of units, the affordable developer responsible for each affordable housing project, and the anticipated year of construction start:
 URL: <https://www.stpaul.gov/departments/planning-and-economic-development/planning/ford-site-highland-bridge/ford-site-2>
 - Ryan Companies did not acquire the CP Rail parcel alongside the Ford Site in 2019 as there were no viable uses for the site at that time, along with the fact that the site has a variety of physical and regulatory challenges. The various challenges make development for alternate uses difficult. A federal and local flight overlay zone limits the ability to provide buildings with occupiable uses (as seen in the supplemental information provided). The westerly portion of the site that is not subject to the airport overlay is



restricted by the Mississippi River Critical Corridor Area. Further compounding development challenges include substantial grade changes on the site, visibility from major roadways and site access.



UST Ballfields Master Plan Amendments Planning Commission Public Comment Visual Supplement

April 28, 2023



University of St. Thomas Ballfields

Project Overview



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HIGHLAND
Bridge Location



Current Development
Proposal: University of
St Thomas Ballfields

HIGHLAND *Bridge* Key Approvals & Milestones



City Approvals

- AUAR Update: Jan 2023 Start, Jun 2023 Complete
- Master Plan Amendments: Jan 2023 Start, Aug 2023 Complete
- Parkland Swap & Dedication: Jan 2023 Start, Aug 2023 Complete
- Legal Documents: Jan 2023 Start, Aug 2023 Complete
- Plat: Mar 2023 Start, Jun 2023 Complete
- Zoning Applications: TBD

Other Approvals

- Airport Overlay Approvals: Jan 2023 Start, June 2023 Complete
- Wetland Approvals: Jan 2023 Start, June 2023 Complete

HIGHLAND *Bridge* Community Engagement



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Community Meetings – Past

- [Highland District Council Community Development Committee January 18, 2022](#)
- [Highland District Council Community Development Committee February 15, 2022](#)
- [Highland District Council Community Development Committee May 17, 2022](#)
- Highland Business Association Lunch and Learn June 13, 2022
- [Highland District Council Community Development Committee June 21, 2022](#)
- [Highland District Council Community Development Committee July 19, 2022](#)
- [Community Meeting August 22, 2022](#)
- [Highland District Council Community Development Committee October 18, 2022](#)
- [Highland District Council Community Development Committee January 24, 2023](#)
- [Highland District Council Community Development Committee February 21, 2023](#)
- [Community Meeting February 28, 2023 at Gloria Dei](#)
- [Highland District Council Community Development Committee March 21, 2023](#)

Individual/Group Neighbor Engagement – Past

- March 2023
- April 2023
- Email correspondence with 100+ Community meeting attendees

Community Meetings – Future Potential

- Highland District Council Community Development Committee – May, June etc. 2023



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HDC Resolution Master Plan Amendments at Highland Bridge

WHEREAS Ryan Companies has submitted an application to amend the Master Plan for the Highland Bridge site in order for the University of St. Thomas (UST) to develop a sports complex on the southern portion of the site; and

WHEREAS the Community Development Committee of the Highland District Council (HDC) has met with Ryan Companies, UST and neighbors on May 17, 2022, June 21, 2022, July 15, 2022, October 6, 2022, Jan. 24, 2023, Feb. 21, 2023 and March 21, 2023 to discuss the proposed plans for UST to complete one a portion of the Highland Bridge site, and 13 acres of the Canadian Pacific (CP) rail yard; and

WHEREAS the proposed sports complex is a use that was originally intended for the southern portion of the Highland Bridge site (2017 Master Plan design), but was changed when there was not a partner to develop the space; and

WHEREAS the City and Ryan have identified the need to amend the language of the Master Plan for this use to be built; and

- WHEREAS six amendments to the Highland Bridge Master Plan are proposed:
- Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses
 - Include civic & institutional uses as a separate land use category with maximum parking per Sec. 63.207 of the Zoning Code
 - Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasteary F6 district
 - Revise the details of the Shared Transportation Corridor to allow flexibility for routing and engineering decision as design progresses;
 - Relocate Park through parkland diversion slightly to the east of th current location. Address parkland diversion benefits package due to the development of the CP parcel.

WHEREAS the construction of ball fields will not meet the FAR because they are not as dense as an office building or other use; and

WHEREAS parking in the original plan was based on more of an urban use and limits the parking on the site to one space per 200 square feet of building, and this calculation does not work for a sports arena, which is based on seat count; and

Resolution 2023-05D



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WHEREAS the construction of surface parking will benefit other groups, such as Highland Park Little League, and neighbors that use the other facilities; the parks, dog park, walking trails; and

WHEREAS the immediate neighbors to the south of the proposed plan have requested at numerous meetings that the proposed parking be a surface lot that does not impact their homes with light pollution, that the parking be recessed, and that sufficient foliage be added to minimize the impact; and

WHEREAS the Master Plan currently requires no more than a 15 foot setback, which applies to a more urban development, but with ball fields would not allow for proper operation, an entry plaza, and a proper setback from vehicle and pedestrian traffic; and

WHEREAS it is important to the community to maintain the Shared Transportation Corridor for future pedestrian/bike/transit uses, the committee encourages allowing for flexibility in the location of the corridor when planning for transit nodes, accessibility to bike trails and walking paths; and

WHEREAS the relocation of Parkland through a parkland diversion and the dedication of parkland benefits due to the development of the CP rail property, will allow for more usable space in Mica Park by taking away the challenge of the grade difference and wetland area of the currently proposed park; and

WHEREAS the neighbors and committee believe this project is making good use of land that has limited uses, would allow for further acquisition and use of the CP Rail Spur, and with the airport overlay and ball grading, this is one of the best uses of this land; therefore

BE IT RESOLVED that the Community Development Committee of the Highland District Council recommends the approval of the following changes to the Highland Bridge Master Plan:

- Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses
- Include civic & institutional uses as a separate land use category with maximum parking per Sec. 63.207 of the Zoning Code
- Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasteary F6 district
- Exempt civic & institutional buildings from the ROW setback in the F6 district
- Revise the details of the Shared Transportation Corridor to allow flexibility for routing and engineering decision as design progresses
- Relocate Park through parkland diversion slightly to the east of the current location. Address parkland diversion benefits package due to the development of the CP parcel.

Resolution 2023-05D



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Approved March 21, 2023
By the Community Development Committee of the Highland District Council

Resolution 2023-05D



Community Access

HIGHLAND *Bridge* Community Benefits



Redevelopment of CP Rail Site: Providing a market identified use, allowing for cleaning up the site and bringing quality facilities and investment to the property, which otherwise will remain vacant and unutilized.

Community Facility Use: Creation of quality facilities that could be used for youth and community ballfield use, entertainment and parking.

Master Plan Alignment: Identifies options for meeting institutional uses as desired in the Master Plan.

Enhanced Parkland: Increase in usable area for existing public parkland and increase in public park value and City benefits package per CP Rail park dedication.

Multimodal Transportation: Unlocks the potential for a future Ford Spur connection, enhancing trail and transit connections. Expanding pedestrian and bike connections through the site and allowing the possibility of future multimodal ped/bike/transit corridor to the east.

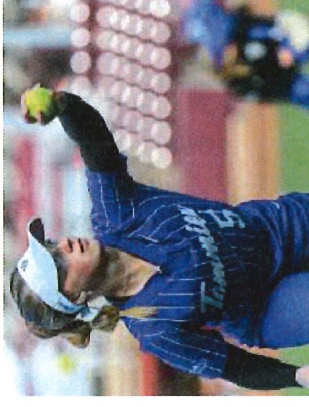
Improved Water Quality: Improving stormwater quality and management on a currently unmanaged site, decreasing flow rate to Hidden Falls Park and improving area wetland conditions.

Sustainability: LEED Silver per UST standards.

HIGHLAND *Bridge* Baseball & Softball Schedule



- Regular practice begins **September through October**
 - **2-3 home games for both baseball/softball**
- Spring outdoor season starts when the snow melts
- Home games **start last week of March end in May** for regular season
 - Both Baseball/Softball will play **up to 20 home games**
- Baseball Conference games are **Friday, Saturday, Sunday**
 - (few mid-week games on a Tuesday or Wednesday evening)
- Softball conference games are normally played on **Saturday (double-header) and Sunday**.
 - (few mid-week games on a Tuesday or Wednesday evening)
- NCAA or Conference postseason games into early June (contingent)
- **Community, High School and Youth sports games variable**
- Event-focused traffic will be ~50 times per year for UST baseball/softball. Other anticipated traffic will be for community events, such as youth sports.





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Parking



- Public access when not in use for UST facilities events. Will support nearby little league fields and adjacent Unčí Makhá and Míča parks.
- Lack of current demand for structured parking based on attendance. As program grows attendance may grow, but use of single occupant vehicles may change
- Alternative transportation approaches to limit vehicular use including pedestrian and bike connections to the site and the potential for shuttle services for athletes and students
- Future potential for shared parking as the adjacent Ryan Block 34 project is developed, should the uses be compatible
- Maintaining flexibility around future shared transportation corridor
- Neighborhood feedback desiring less visible parking and more tree cover and screening



Rendered Site Plan



Initial Build – Surface Parking

Site Plan is Preliminary and subject to change



Potential Future Build – Parking Ramp



Rendered Site Plan

Site Plan is Preliminary and subject to change



Rendered Site Plan

Site Plan is Preliminary and subject to change



View From Hampshire Ave – No Landscape Screening (Looking North)



Initial Build – Surface Parking

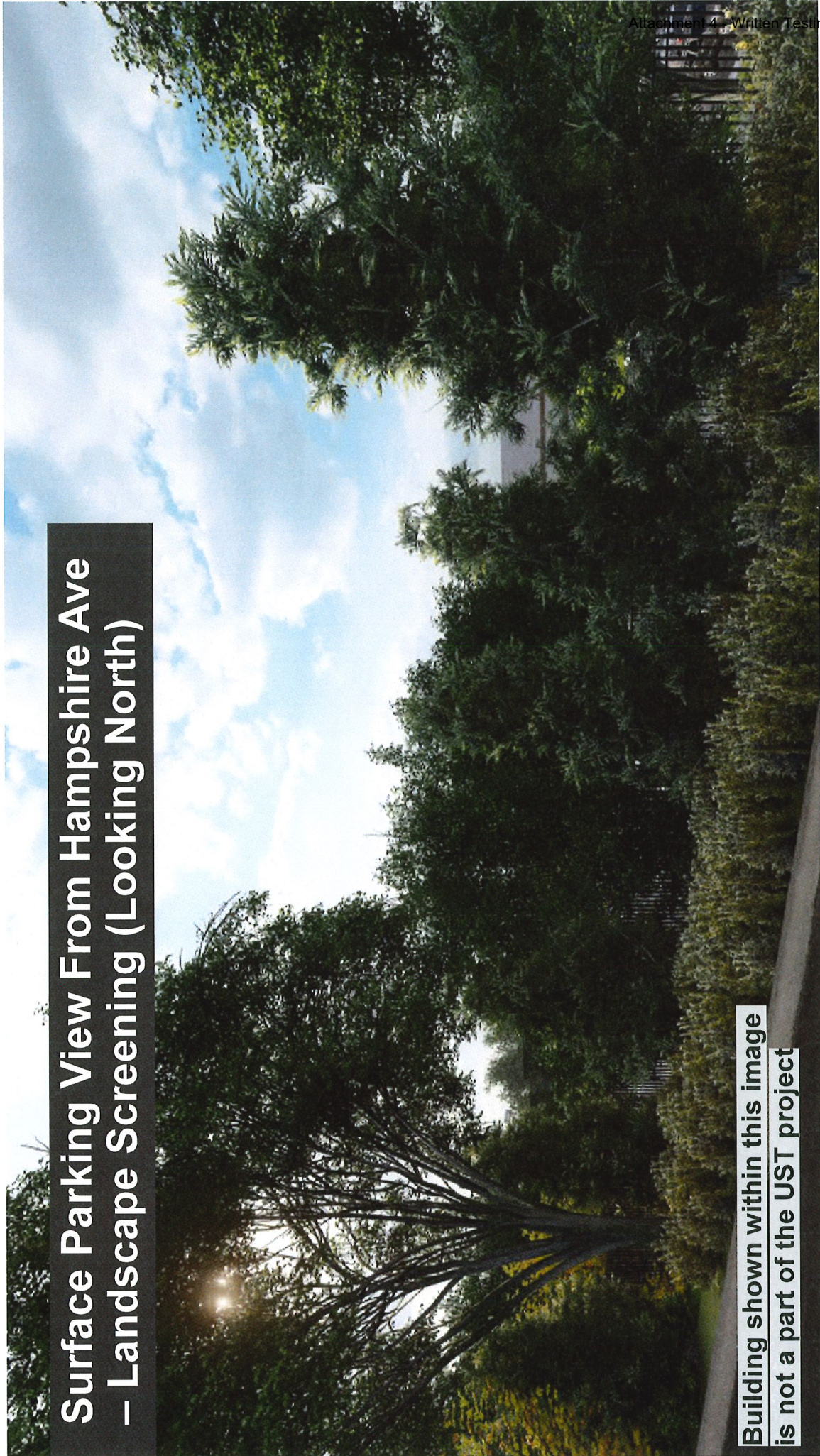


Potential Future Build – Parking Ramp

**Surface Parking View From Hampshire Ave --
No Landscape Screening (Looking North)**



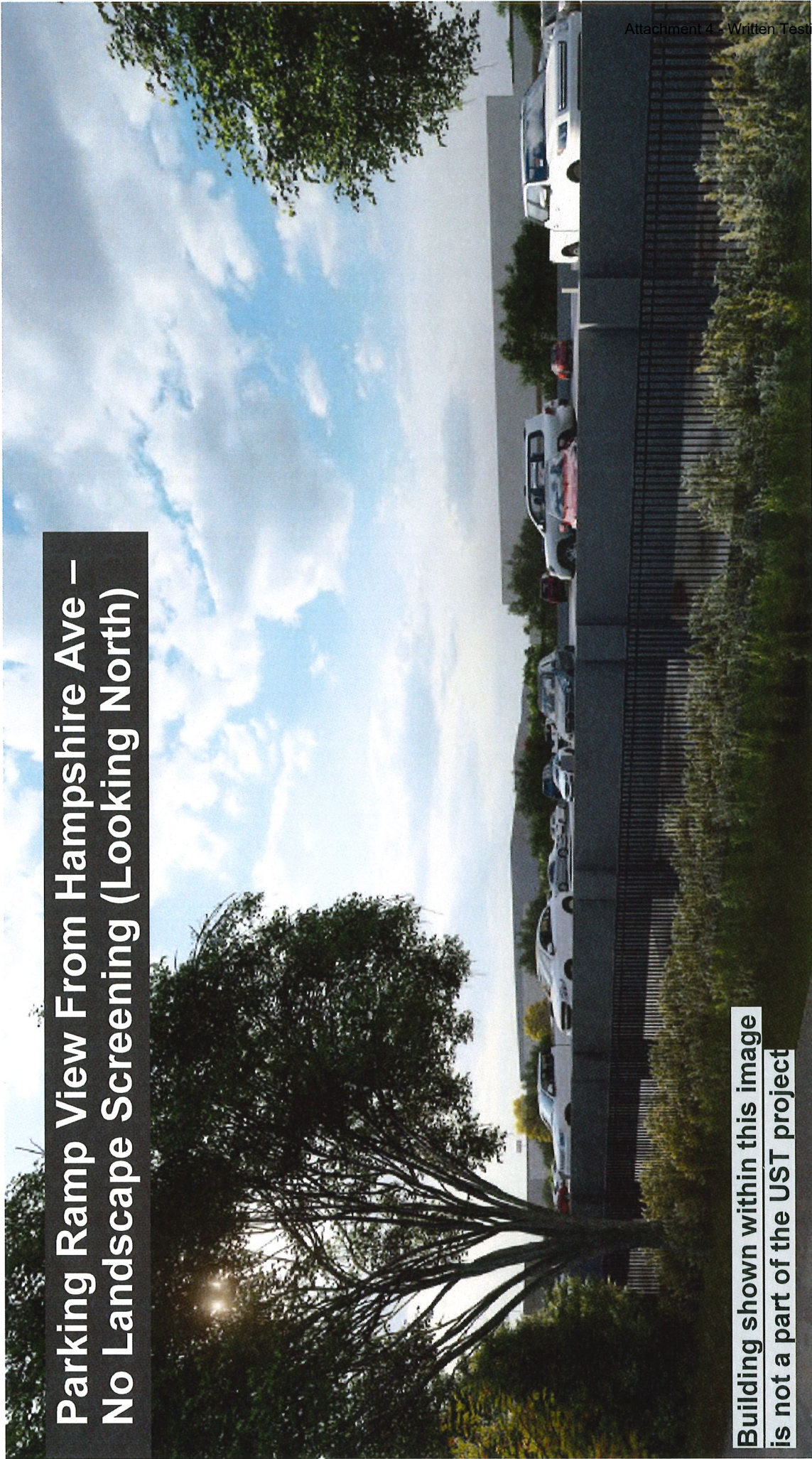
**Building shown within this image
is not a part of the UST project**



**Surface Parking View From Hampshire Ave
– Landscape Screening (Looking North)**

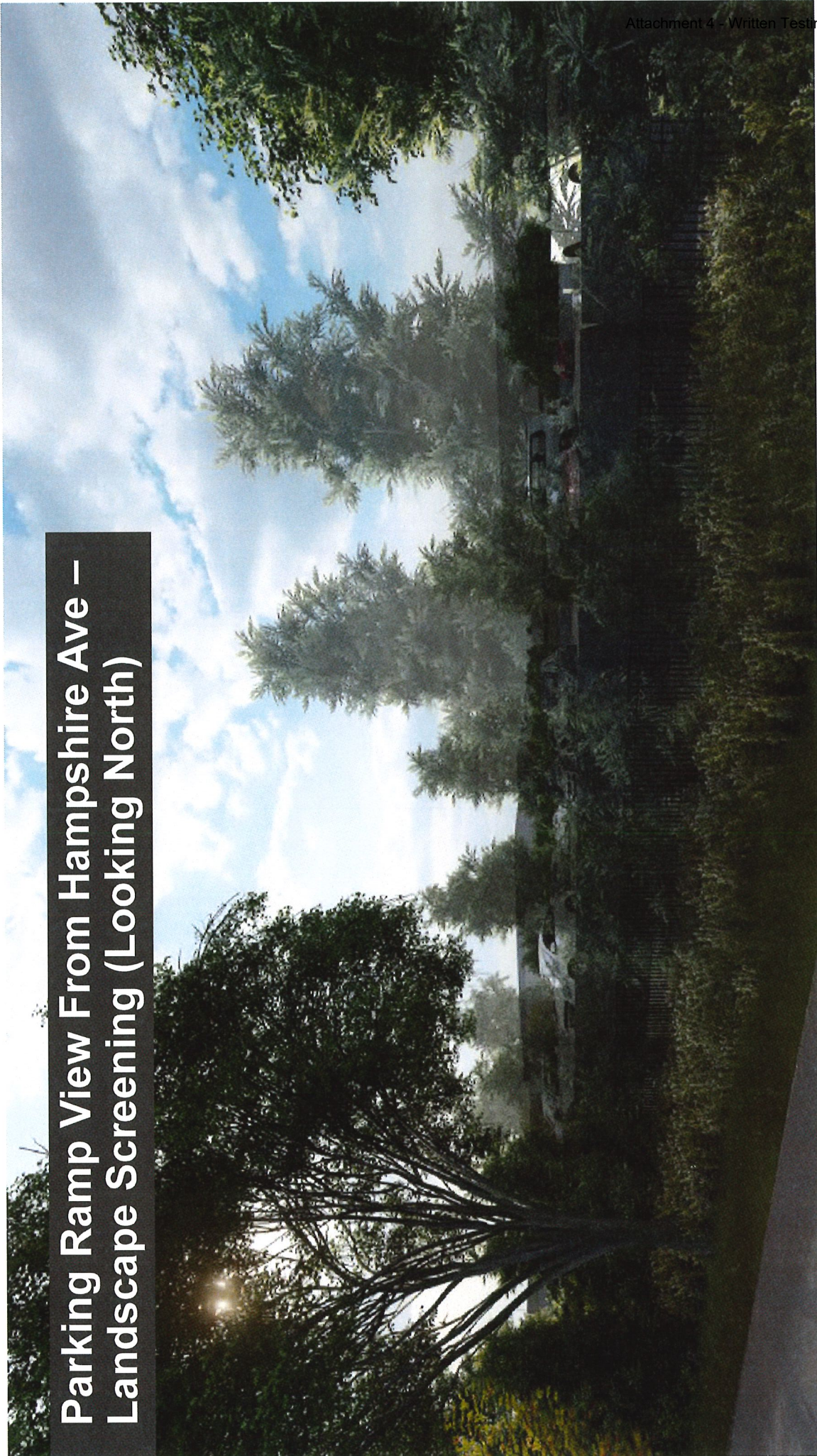
**Building shown within this image
is not a part of the UST project**

**Parking Ramp View From Hampshire Ave -
No Landscape Screening (Looking North)**



**Building shown within this image
is not a part of the UST project**

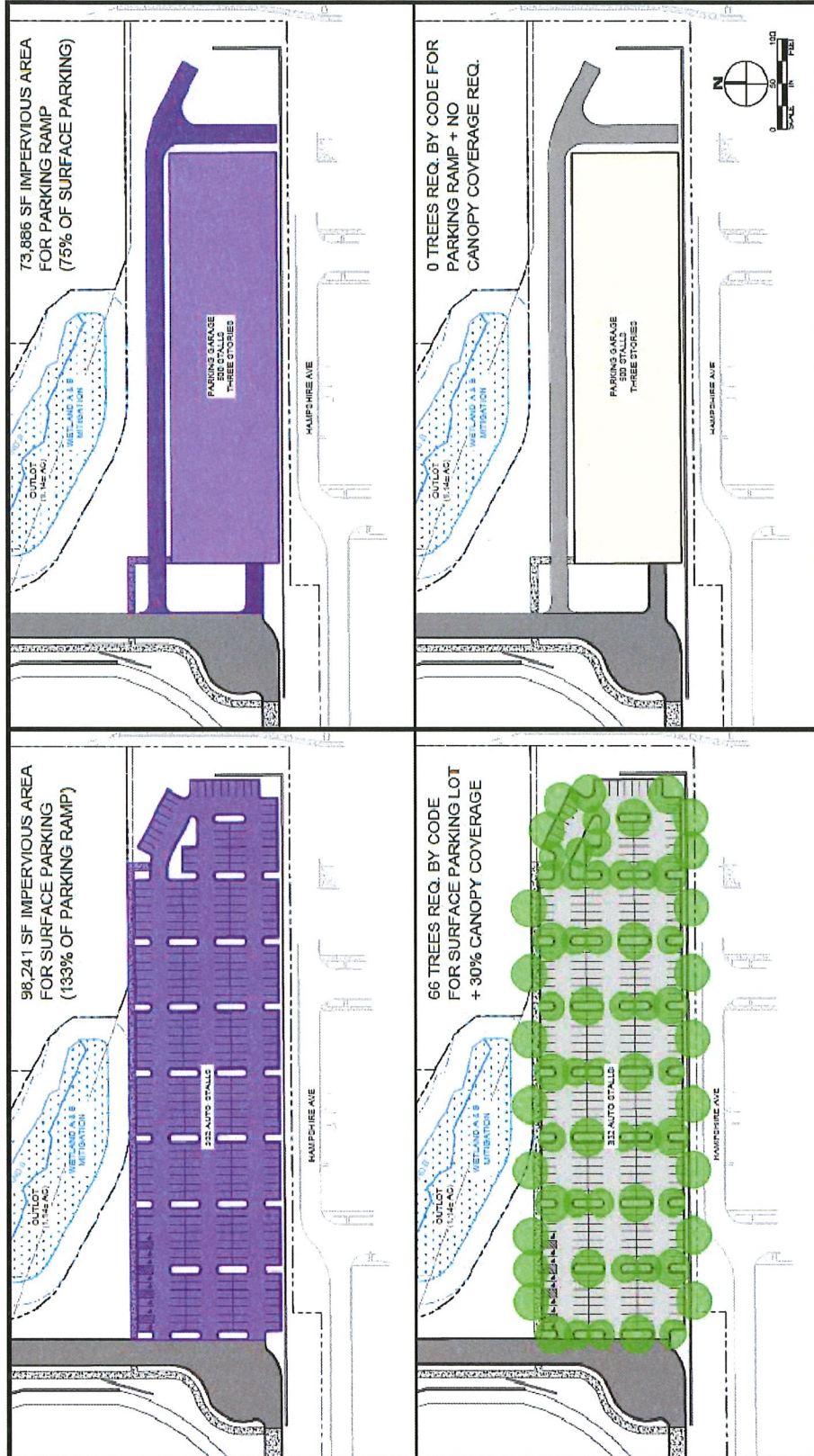
**Parking Ramp View From Hampshire Ave --
Landscape Screening (Looking North)**





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HIGHLAND
Bridge



Potential Future Build – Parking Ramp

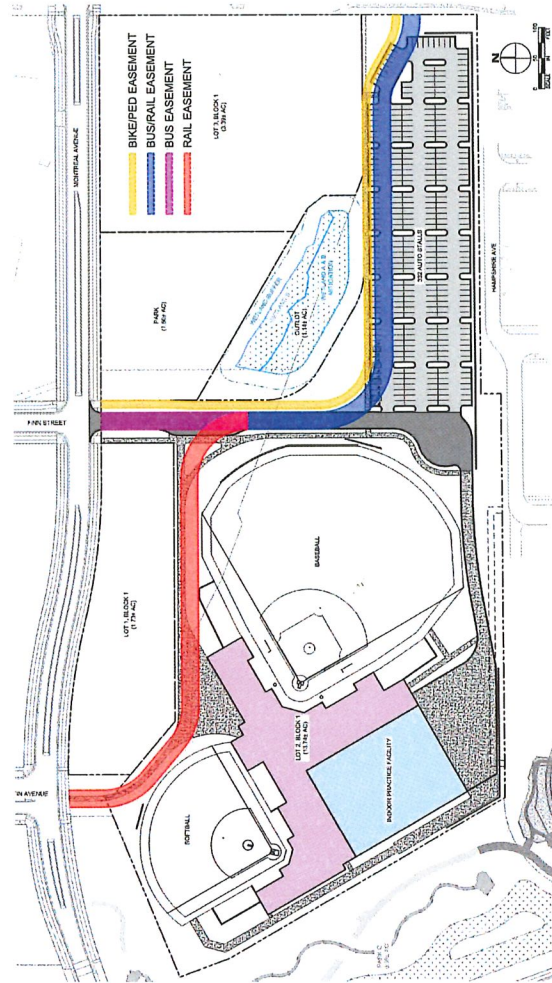
Initial Build – Surface Parking



Shared Transportation Corridor

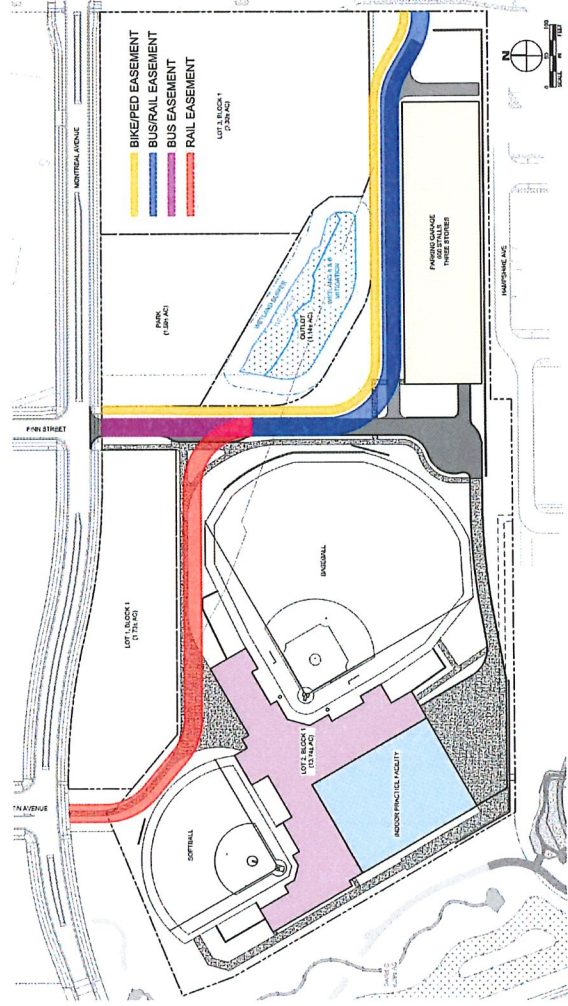


Transportation Corridor



Initial Build – Surface Parking

Site Plan is Preliminary and subject to change

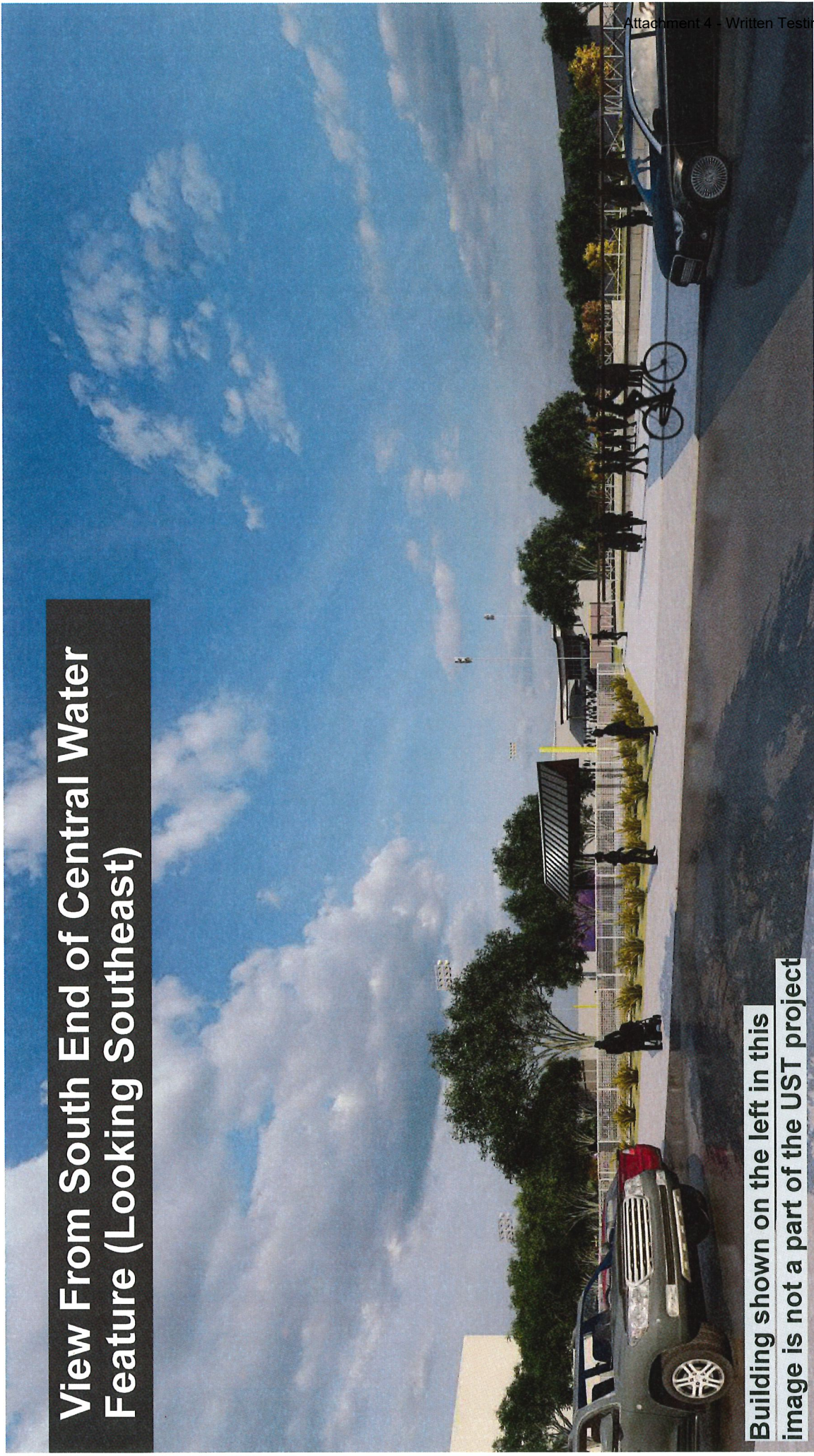


Potential Future Build – Parking Ramp



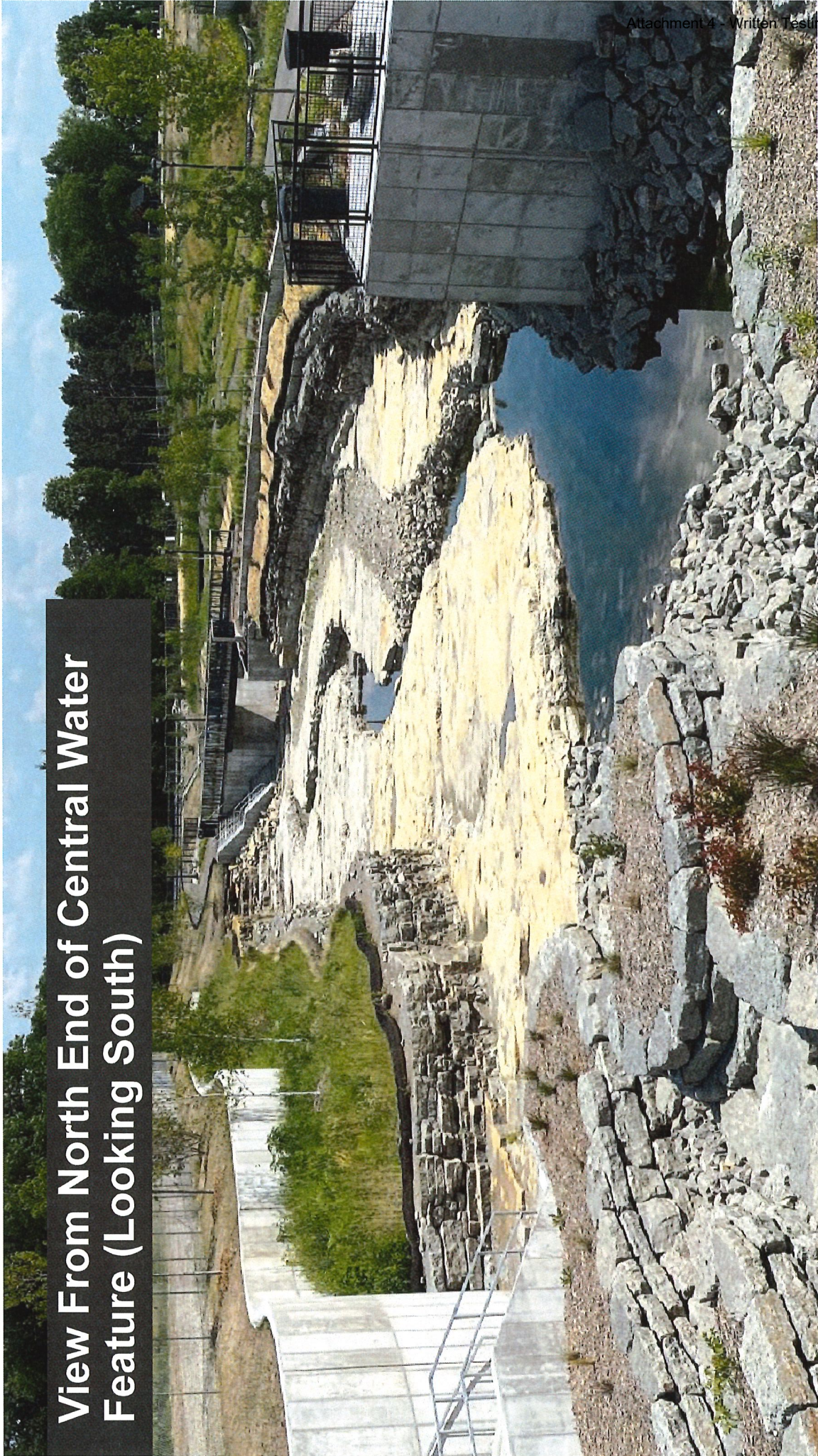
Relationship with Unčič Makhá Park

**View From South End of Central Water
Feature (Looking Southeast)**



**Building shown on the left in this
image is not a part of the UST project**

**View From North End of Central Water
Feature (Looking South)**

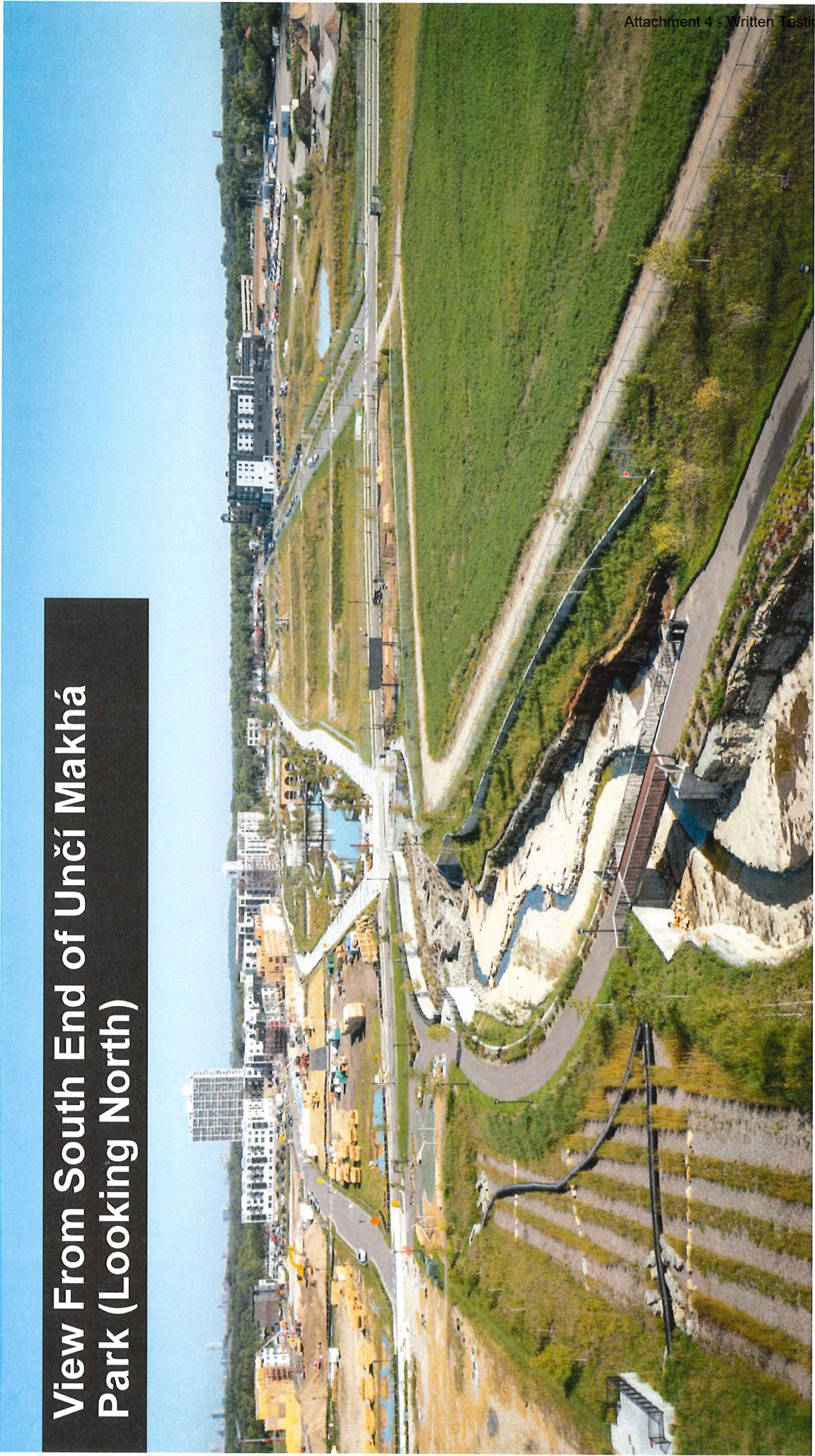


**View From South End of Unčí Makhá
Park (Looking Northeast)**



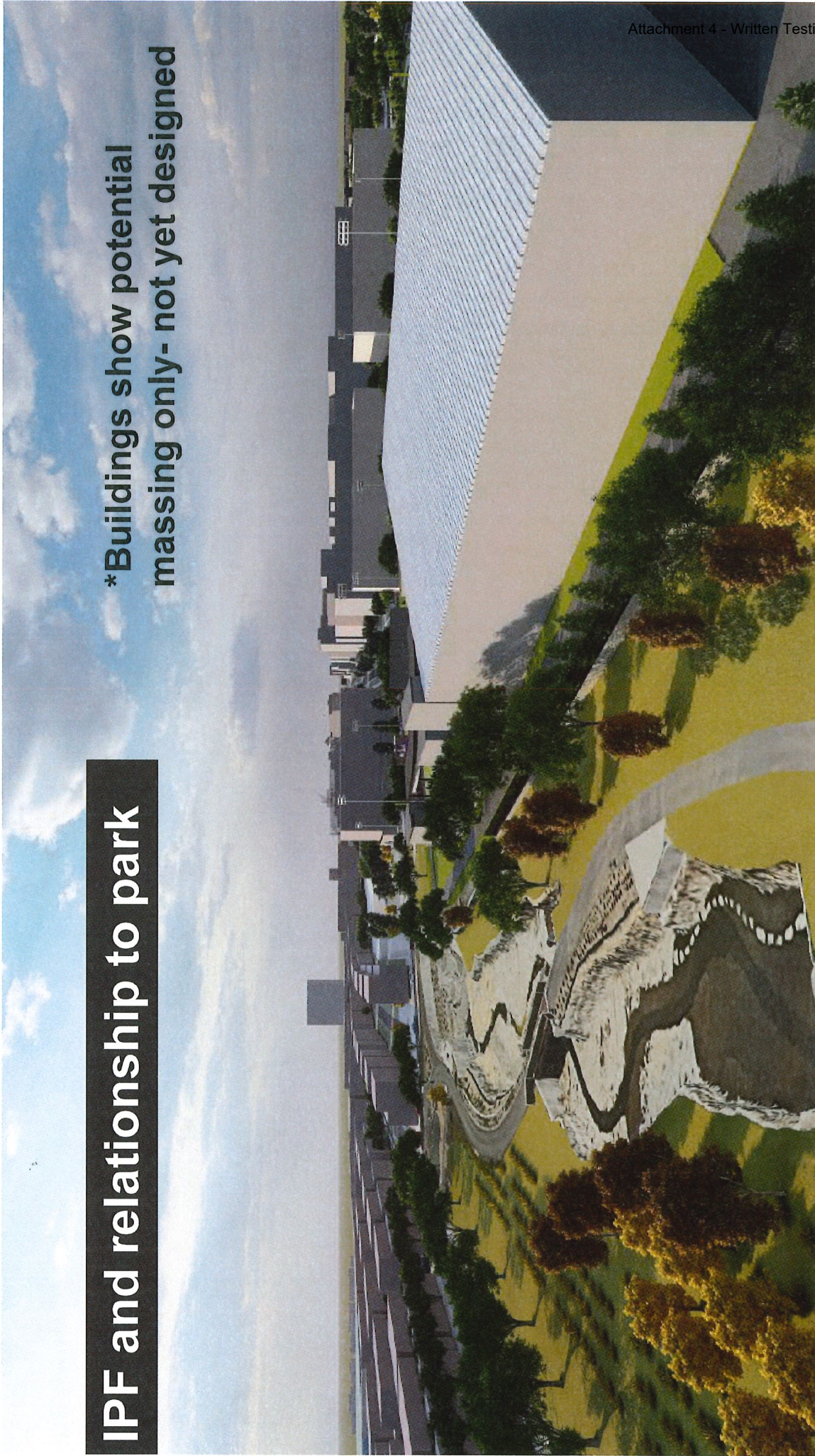
Buildings show potential
massing only- not yet designed

**View From South End of Unčí Makhá
Park (Looking North)**



IPF and relationship to park

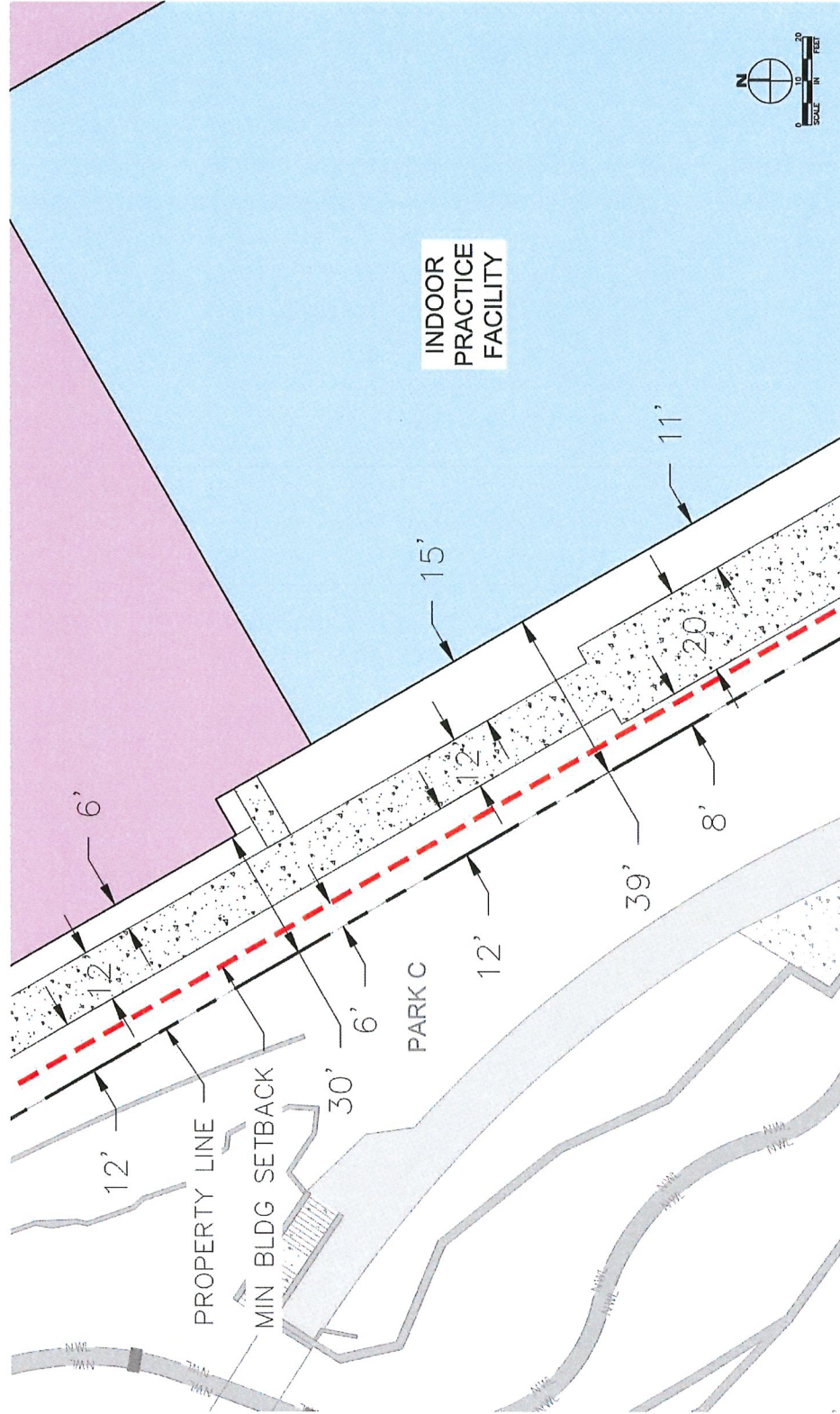
*Buildings show potential massing only- not yet designed





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HIGHLAND
Bridge Building Minimum Setback



Minimum Building Setback



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Design of Building/Height





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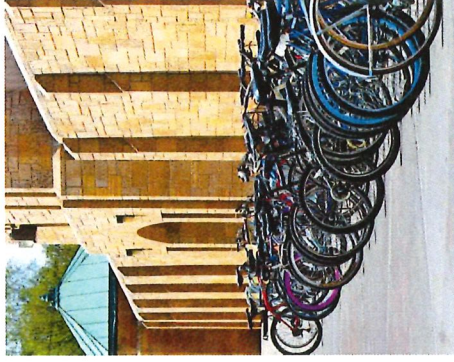
HIGHLAND
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Sustainability

HIGHLAND *Bridge* Sustainability



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The University of St. Thomas is planning on constructing to LEED silver standards. The University has solar on many on-campus buildings and invests in solar research through their Center for MicroGrid Research.





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Key Approvals and Milestones

HIGHLAND *Bridge* Key Takeaways



- Present zoning in the **F6 Gateway District** allows for **recreational facilities and institutional uses**. However, the Master Plan was not created with specific design requirements of recreational facilities in mind, therefore Master Plan Amendments are needed.
- All Master Plan Amendments must be approved to allow for the functionality of the ballfields.
- Benefits of the UST Ballfields include:
 - Opportunity to put CP Rail site into a productive use, satisfying an unknown of what might develop there, cleaning up the site and **bringing investment** as well as **unlocking multimodal ped/bike/transit connections** to allow for the Ford Spur connection.
 - Creation of **quality facilities** that could be used for youth and community ballfield use and parking.
 - **Increase in usable area for existing public parkland** and increase in public park value per CP Rail park dedication.



Additional Supplemental Information



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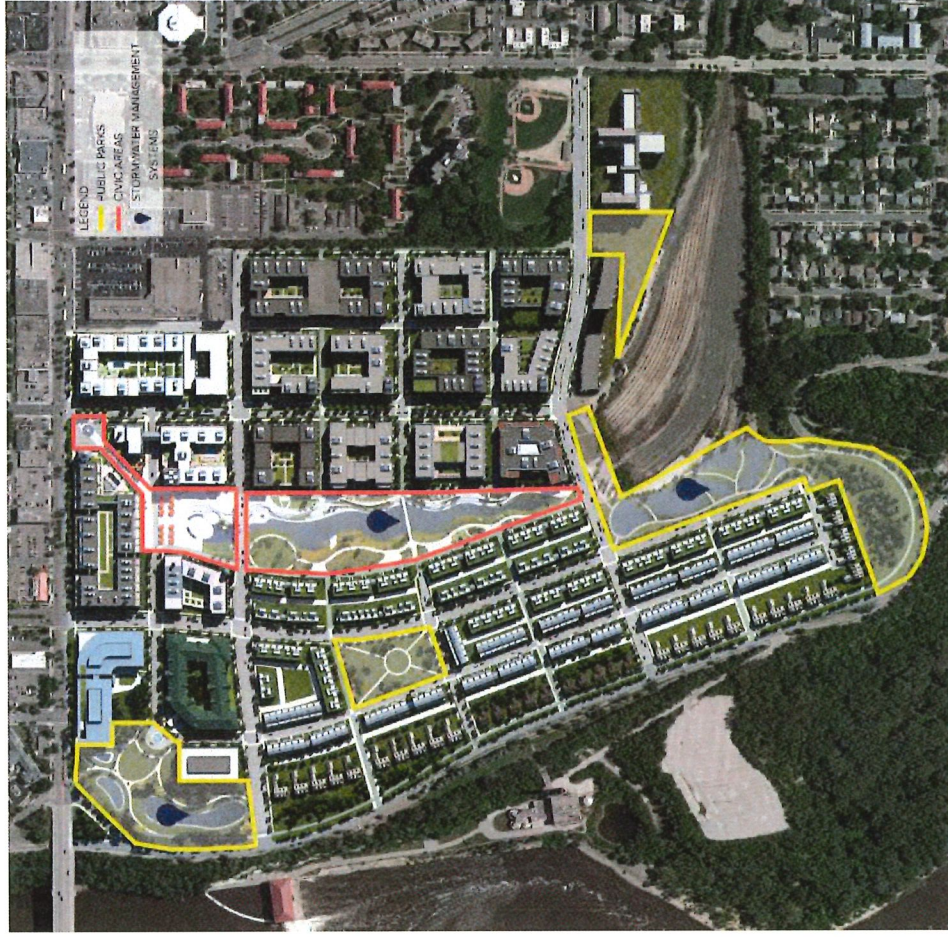
Parkland

HIGHLAND *Bridge* Parkland Diversion



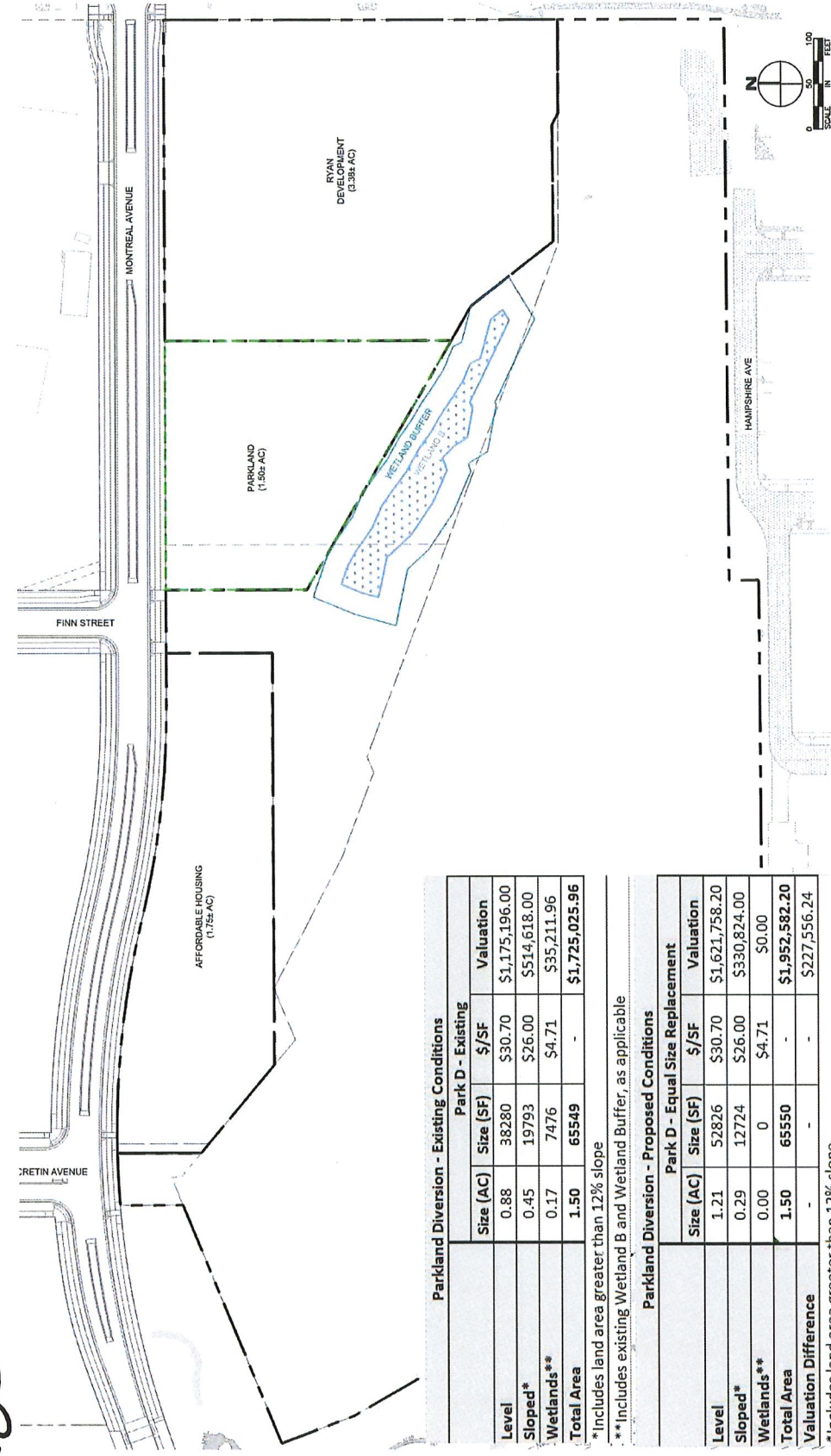
Parkland Diversion

- Park D, or Miča Park, land area (1.50 acres) is owned by the City of St. Paul Parks and Recreation Department and is currently unimproved land. The park is only ~25% usable due to existing and proposed wetlands and steep slopes within the parcel.
- The ballfields project will provide an opportunity to relocate Miča Park to a 38% more usable site for public use that otherwise would not have been possible. The new park location also increases street frontage/access by 230% from current land-locked park parcel.
- As a part of the parkland diversion process, the City hires an independent appraiser to assess the value of the existing and future land area. This appraisal process has been completed and shows an increased value of the park property of \$227k.





Parkland Diversion - City Recommended Plan



Parkland Diversion - Existing Conditions

	Size (AC)	Size (SF)	\$/SF	Valuation
Level	0.88	38280	\$30.70	\$1,175,196.00
Sloped*	0.45	19793	\$26.00	\$514,618.00
Wetlands**	0.17	7476	\$4.71	\$35,211.96
Total Area	1.50	65549	-	\$1,725,025.96

* Includes land area greater than 12% slope
** Includes existing Wetland B and Wetland Buffer, as applicable

Parkland Diversion - Proposed Conditions

	Size (AC)	Size (SF)	\$/SF	Valuation
Level	1.21	52826	\$30.70	\$1,621,758.20
Sloped*	0.29	12724	\$26.00	\$330,824.00
Wetlands**	0.00	0	\$4.71	\$0.00
Total Area	1.50	65550	-	\$1,952,582.20
Valuation Difference	-	-	-	\$227,556.24

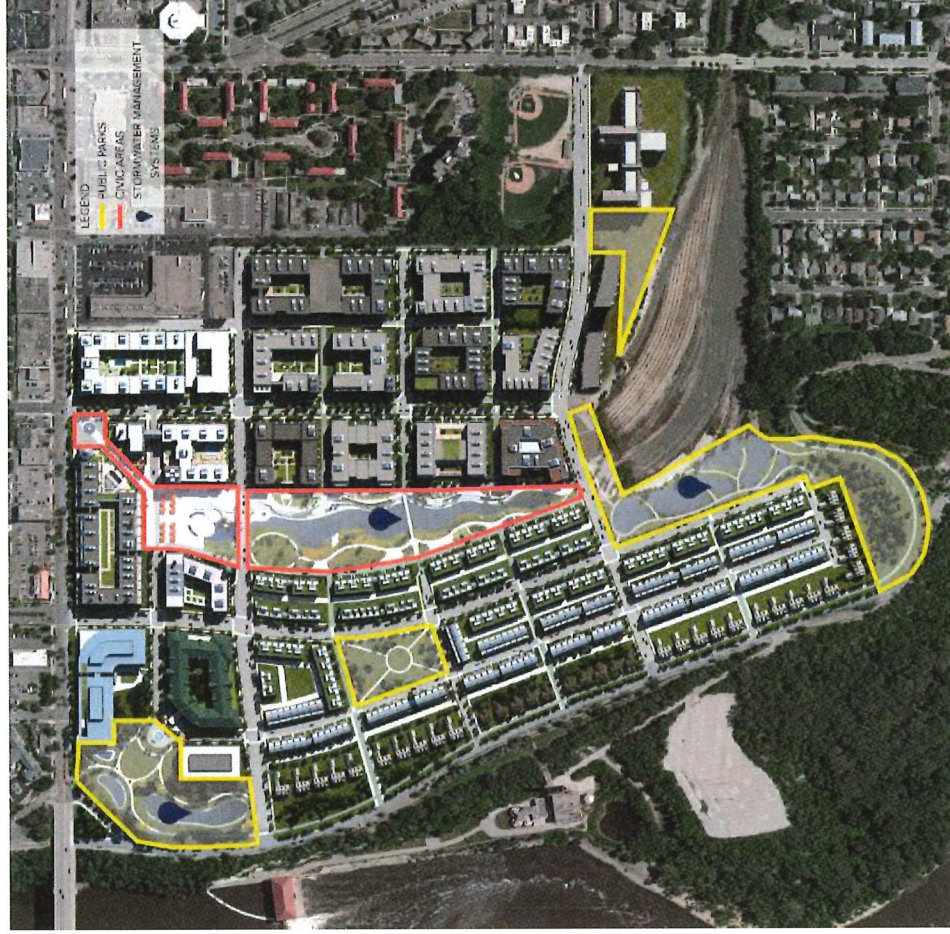
* Includes land area greater than 12% slope
** Includes existing Wetland B and Wetland Buffer, as applicable

HIGHLAND *Bridge* Parkland Dedication



Parkland Dedication

- Parkland Dedication is required for redevelopment of the CP Rail parcels.
- Since Parks Dept is not interested in accepting land with wetlands included, and there are no other viable areas within the CP Rail parcel site to meet the dedication requirements in the form of a land dedication, Parks staff has suggested an alternative means of compliance for parkland dedication, when combined with the parkland diversion package creates a comprehensive, high-value benefits package for the City owned parkland.



HIGHLAND *Bridge* Parkland Dedication

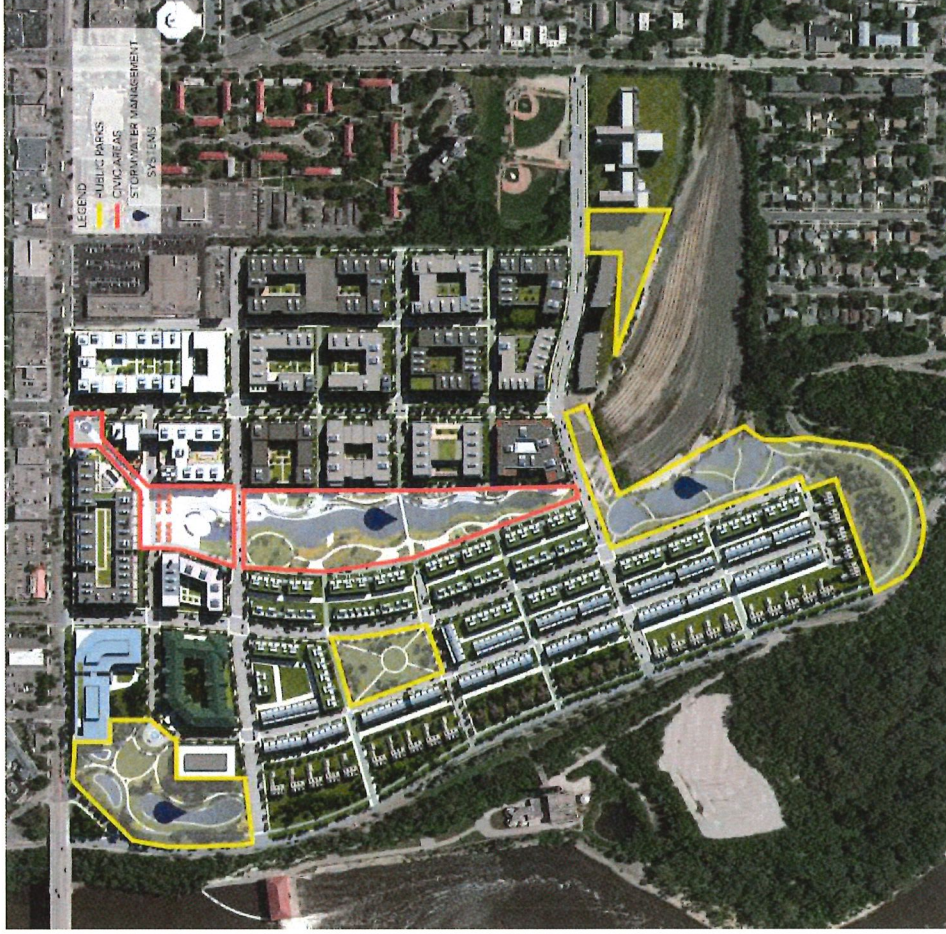
Parkland Dedication

Identified captured value as part of CP Rail Park Dedication:

- Partnerships with community youth sports teams and public and private groups for scheduled, for-fee use of the UST ballfields, which are being constructed with private dollars with an anticipated investment value of ~\$38M.
- Community parking access when not utilized by UST for their games (~50 times/year) or events scheduled by their youth/community partners. This will benefit Highland Ball (serving 1,000 youth athletes) as well as visitors to the nearby City of St Paul public parks. The City is not contributing financially to the building of this ~\$2M parking lot, which will be paid for privately by UST.
- A cash payment of \$250,000, doubling the existing Miça Park budget of \$245,000, providing enhanced amenities for the community.
- Civil engineering costs for improved Miça Park valued at \$10,000.



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Property Taxes

The UST Ballfields is the highest and best use for the site. Without an identified use for the CP Rail site and a user willing to clean up the site, development in this area was unlikely. Additionally, CP Rail site currently has a low tax generation (~\$5k/year in assessments), the CP site is the best place to accommodate an institutional use as it will not change anticipated minimum assessment values on the Highland Bridge site. There is no loss in tax revenue with this new use. The adjacent Ryan developments will be able to be developed to the full extent that was anticipated in the redevelopment agreement.

UST Tax Impact

- A recent study commissioned concluded the economic impact to be over \$400M/year in positive economic impact in St. Paul alone.
- St. Thomas generates ~\$40M annually in state and local taxes through spending by institution, faculty, staff, students and visitors. UST does realize a sales tax benefit on some construction material purchases for development but no other tax benefits on development apply.



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Other Uses



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CP Rail Site is fenced area



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Wetlands

HIGHLAND *Bridge* Wetlands – Existing



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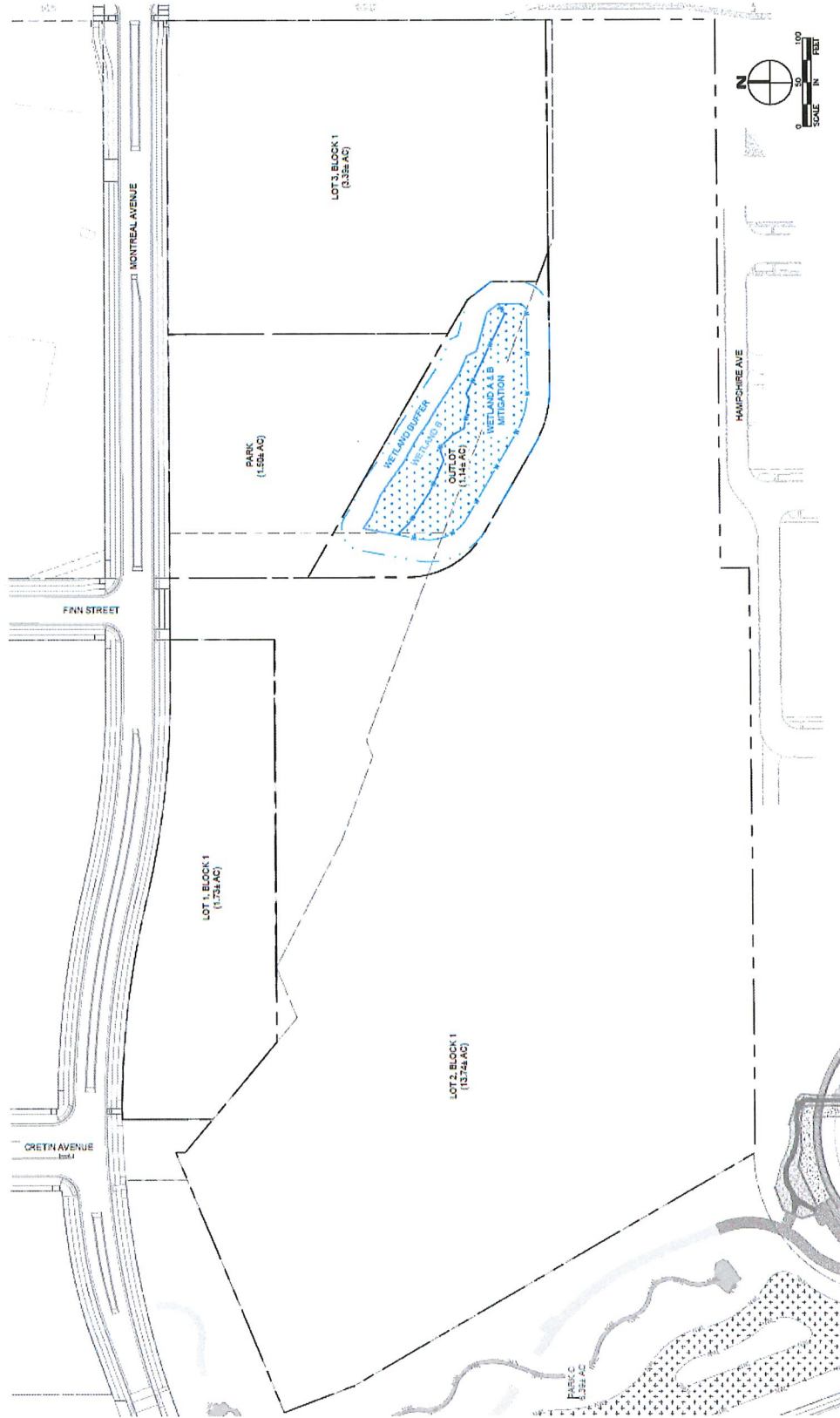
Existing wetlands in the site include the area in blue. Wetland mitigation was originally planned to be added to the west but is now proposed to be added to the south. Wetland delineation completed in 2021 on the CP Rail site indicated existing wetlands were incidental (manmade) drainage areas that were created to manage water around the rail lines.





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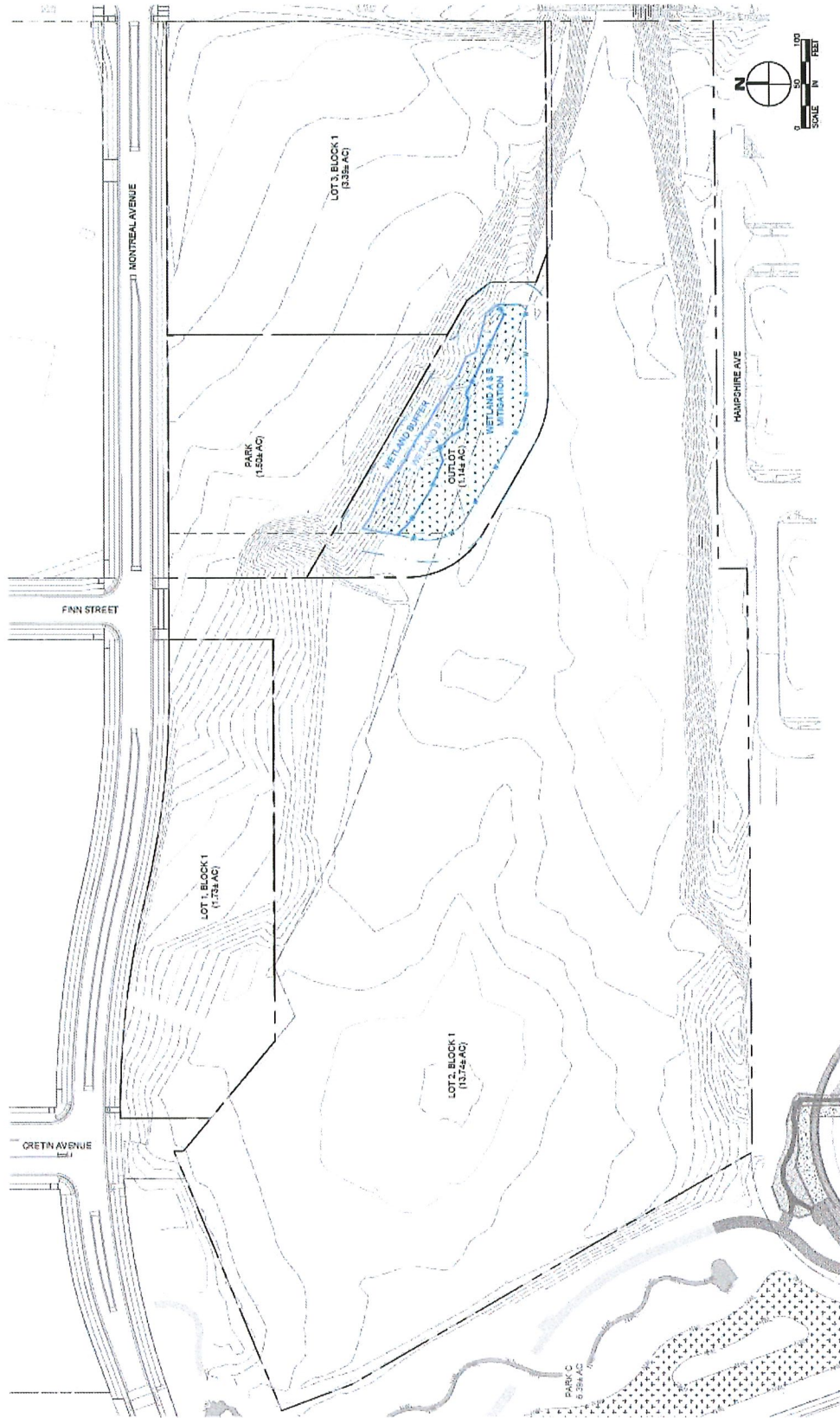
HIGHLAND *Bridge* Wetlands – Proposed





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HIGHLAND *Bridge* Wetlands – Proposed w/grading





Highland Bridge Master Plan

Proposed Amendments

HIGHLAND *Bridge* Master Plan Amendments



MP Amendment 1: Floor Area Ratio	- Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses
MP Amendment 2: Parking	- Modification to Table 4.7 for Civic & Institutional uses as a land use category and setting the maximum spaces allowed as provided in Section 63.207 of the Zoning Code
MP Amendment 3: Surface Parking	- Exclusion to the maximum surface parking spaces within the southeasterly F6 district for Civic & Institutional uses
MP Amendment 4: Building Type Standards	- Exclusion to the maximum Public Right of Way Setback within the F6 district for Civic & Institutional uses
MP Amendment 5: Shared Transportation Corridor	- Adjustment to the alignment and information within the Shared Transportation Corridor description
MP Amendment 6: Open Space System	- Adjustment to the Open Space System for the proposed relocation of the Community Green Space and CP Rail Parkland Dedication



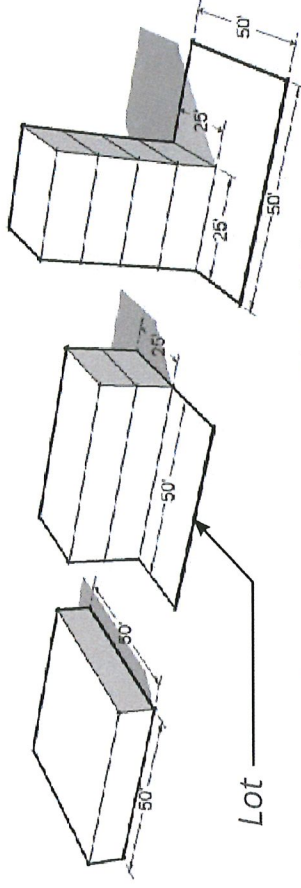
Master Plan Amendment #1

Floor Area Ratio

HIGHLAND *Bridge* Master Plan Amendments



Master Plan Requirement: The Master Plan requires a range of FARs for each of the six zoning districts ranging from 0.25 in the F1 district up to 6.0 in the F4 district. The F6 district where most of the recreational facilities exist in the development or are proposed requires a minimum of 1.0 FAR.



Above examples have an FAR of 1.0

Requested Amendments: Exclusion to FAR requirements within the F6 district for Civic & Institutional uses.

Staff Recommendation: Exclude Civic & Institutional uses/buildings from the minimum FAR requirement in all F districts. Update design standards G17 and G19 for clarity.

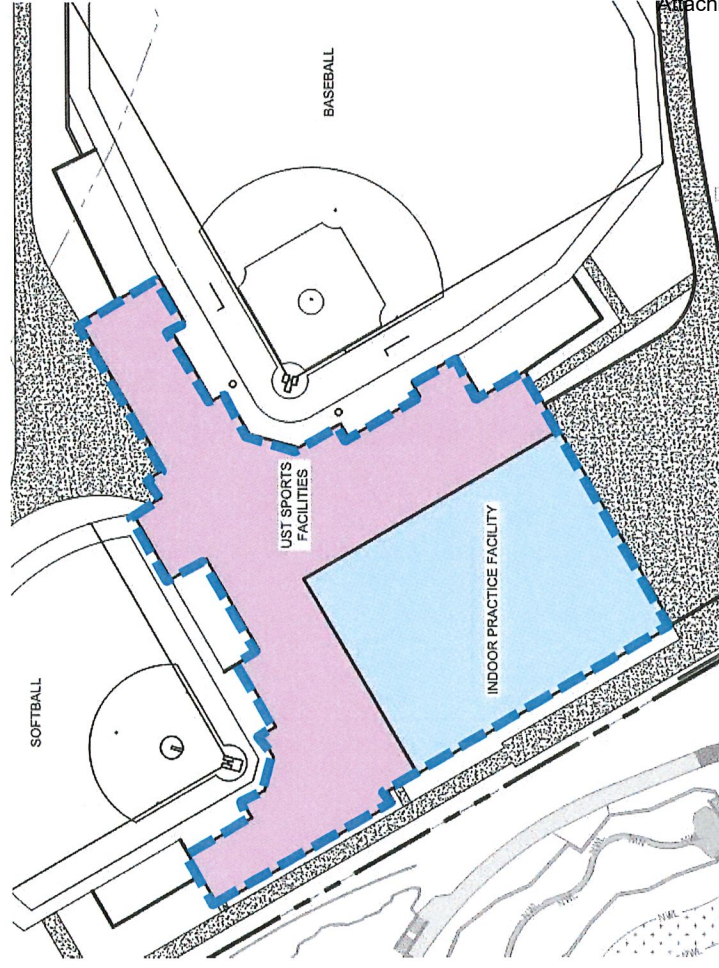


HIGHLAND *Bridge* Master Plan Amendments



Supporting Information:

- FAR is calculated as the total floor area of all buildings or structures on a lot divided by the area of the lot.
- The total floor area calculation excludes recreational field areas such as ballfields or courts, plaza areas, and parking facilities that are often prevalent in Civic & Institutional facilities.
- Civic & Institutional uses are typically only one or two stories in height, thus reducing the ability to achieve higher densities with taller buildings.
- This amendment would benefit the UST Ballfields project as well as the existing Highland Ballfields if those were ever renovated.



— Included in FAR calculation



Master Plan Amendment #2

Parking

HIGHLAND *Bridge* Master Plan Amendments



Master Plan Requirement: The Master Plan sets non-residential parking maximums based on 1 space per 200 square feet of building area.

Requested Amendments: Modification to Table 4.7 for Civic & Institutional uses as a land use category and setting the maximum spaces allowed as provided in Section 63.207 of the Zoning Code.

Staff Recommendation: Include Civic & Institutional uses as a separate land use category with maximum parking thresholds per Sec. 63.207 of the Zoning Code



Table 4.7 Vehicle Parking Requirements by Use Category

Land Use Categories	Maximum Parking	Maximum Spaces Allowed
Non-residential		1 space per 200 square feet GFA
Residential, dwellings		2 spaces per dwelling unit
Residential, congregational living		1 space per bedroom

HIGHLAND *Bridge* Master Plan Amendments



Supporting Information:

- Civic & Institutional uses commonly include spaces of public gathering or activation such as ballfields, play areas, and plazas which don't contribute to floor area in the GFA calculation used by the parking calculations.
- The maximum parking calculation in the Master Plan does not align with the nature of the Civic & Institutional facilities which have a lesser quantity of building GFA but maintain a high demand for parking for attendees/visitors based on seat count.
- St. Paul Zoning Code Section 63.207 does allow for a more accurate calculation of parking spaces based on the specific development use.



Master Plan Amendment #3

Surface Parking

HIGHLAND *Bridge* Master Plan Amendments



Master Plan Requirement: The Master Plan limits the number of surface parking stalls on any given development block to 20 or fewer.

Requested Amendments: Exclusion to the maximum surface parking spaces within the southeasterly F6 district for Civic & Institutional uses only.

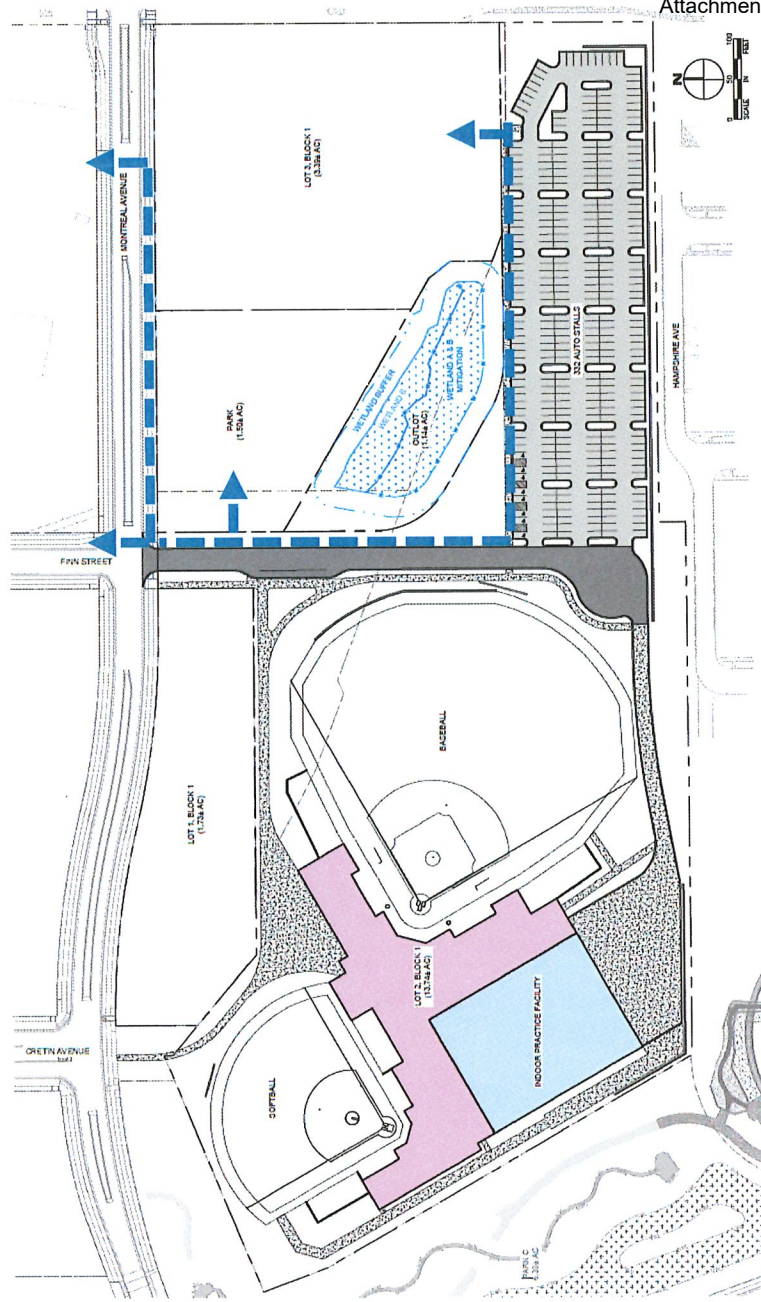
Staff Recommendation: Exempt Civic & Institutional uses from the 20 surface parking space maximum in the southeasterly F6 district.

HIGHLAND *Bridge* Master Plan Amendments

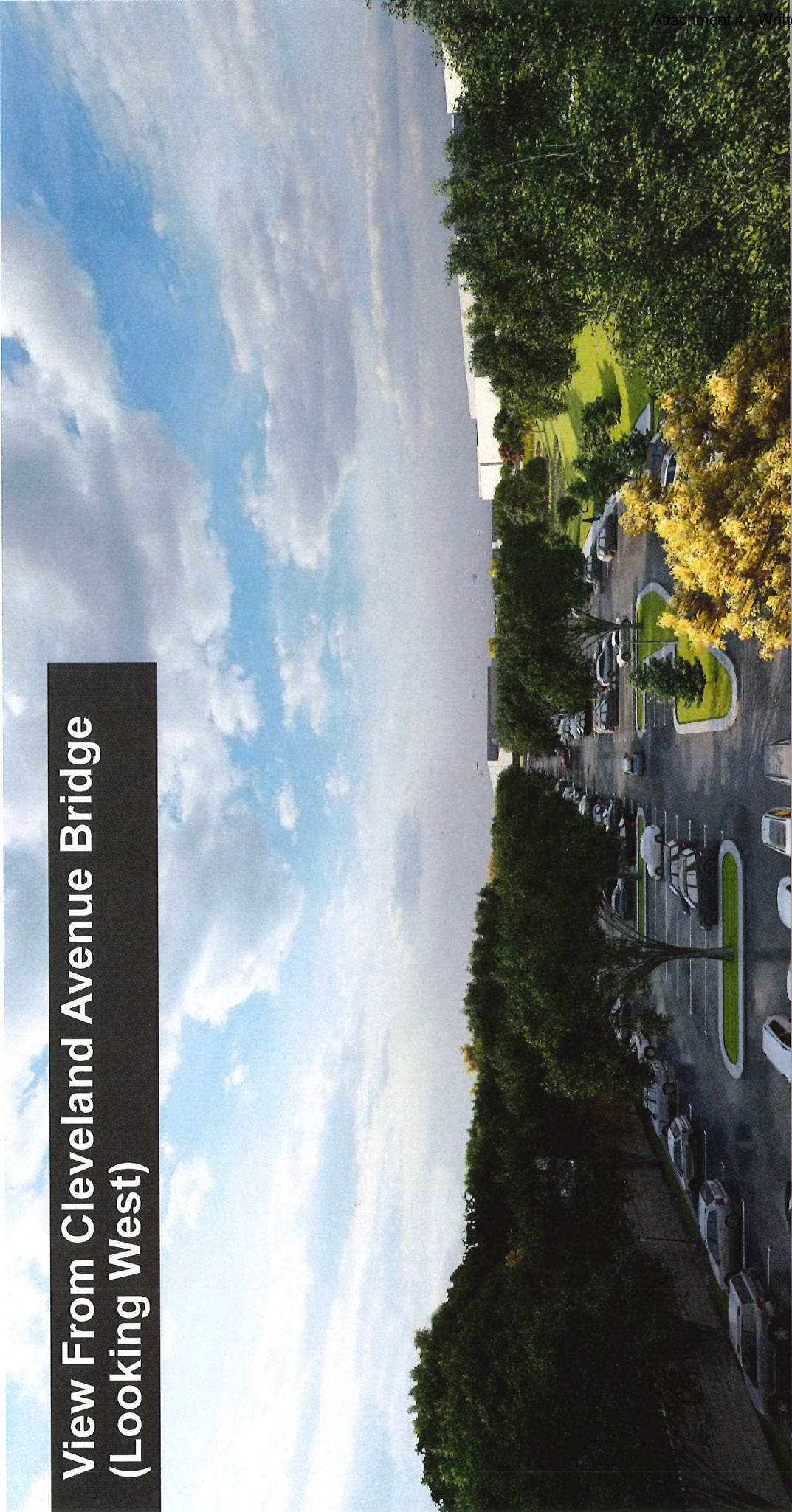


Supporting Information:

- For the UST project proposed in the F6 district, surface parking provides:
 - Flexibility for the quantity of parking based on actual usage and the anticipated growth of the athletic programs
 - Future potential for shared parking and uses with adjacent developments as they get built out in the future. This includes the shared transportation corridor planned to route through the CP Rail parcel.
 - Better views for neighbors including landscaped parking that is set down at a lower elevation as opposed to vertical building façade adjacent to their properties.

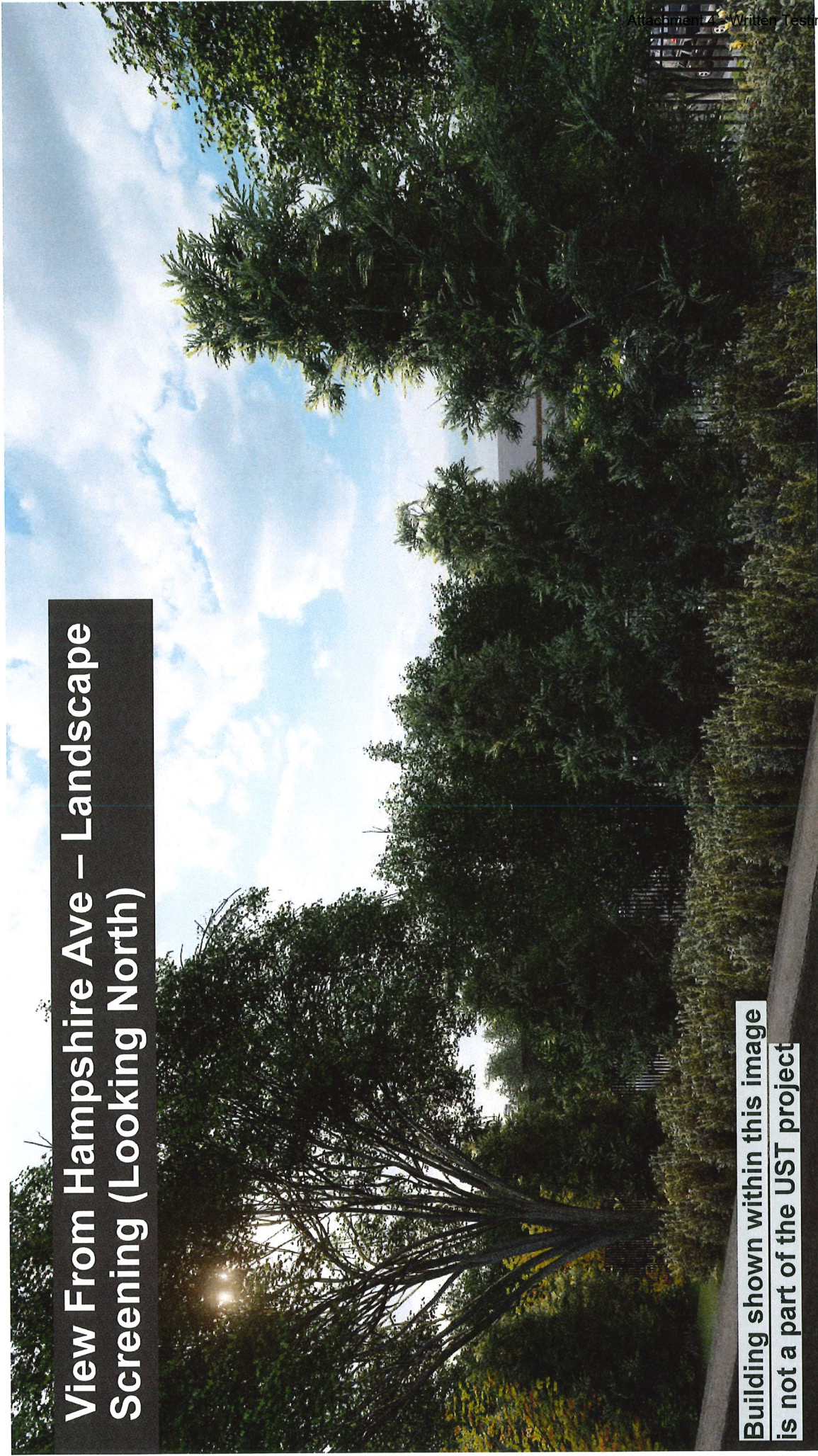


**View From Cleveland Avenue Bridge
(Looking West)**

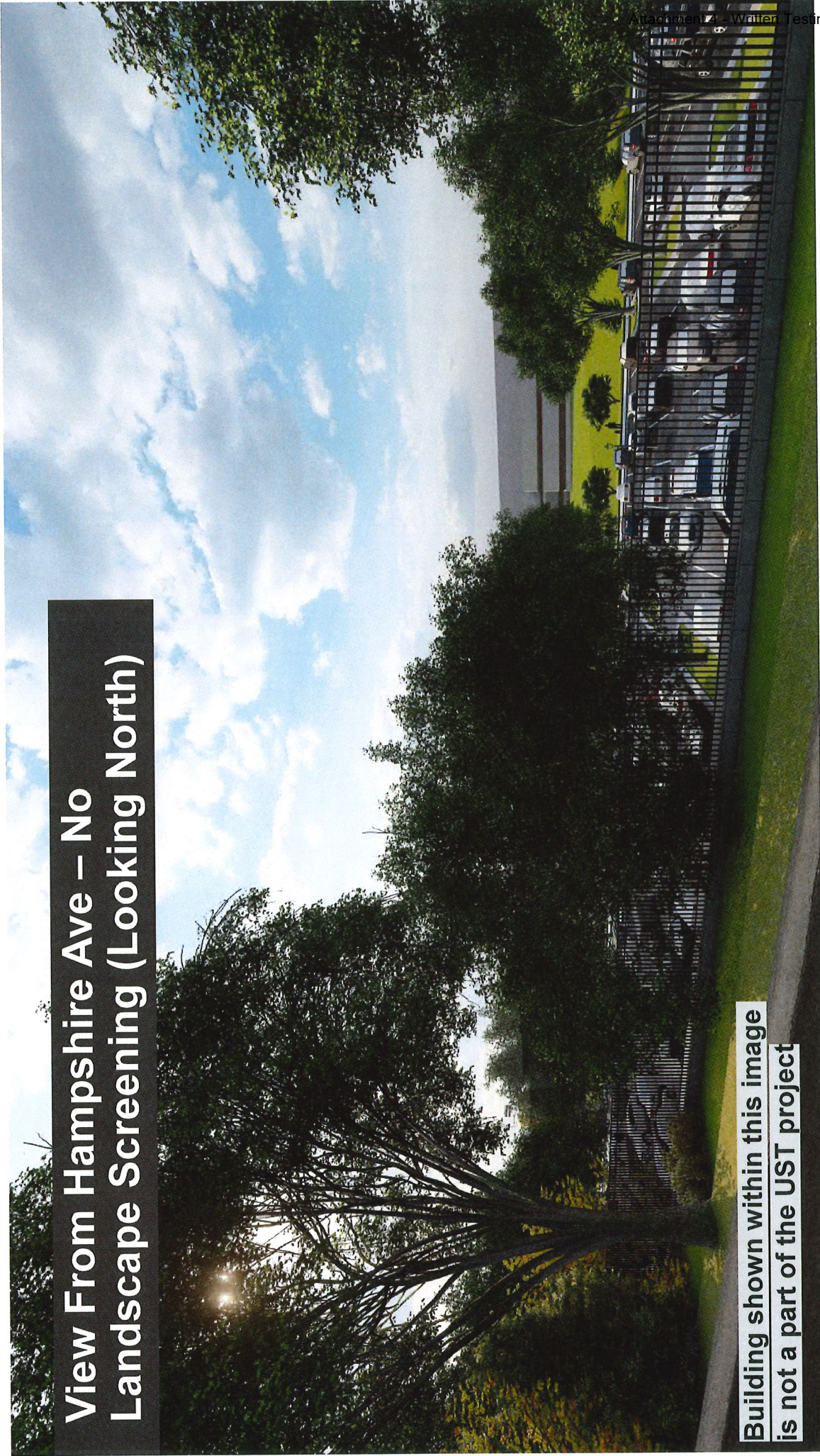


View From Hampshire Ave – Landscape Screening (Looking North)

Building shown within this image is not a part of the UST project

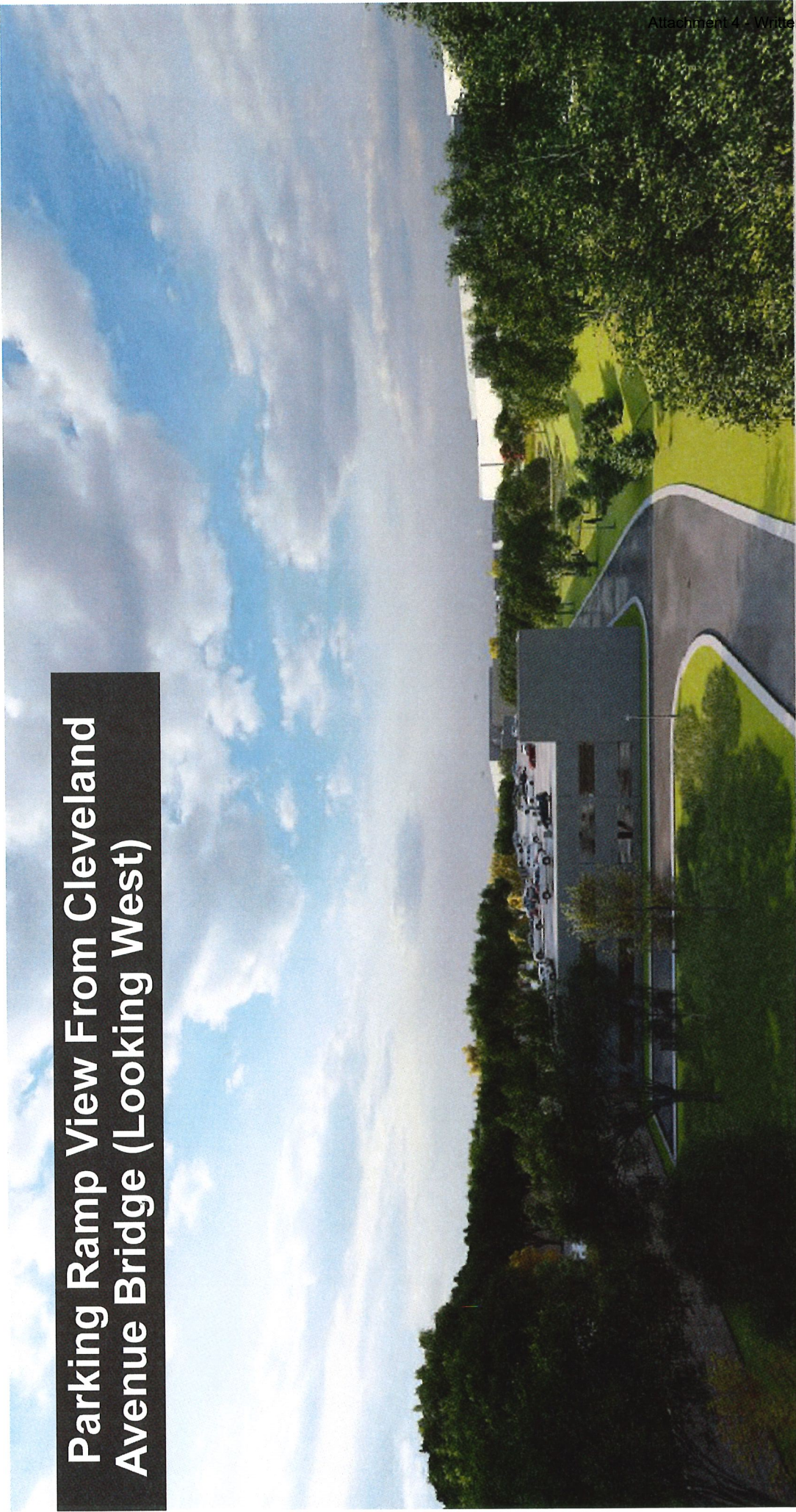


**View From Hampshire Ave – No
Landscape Screening (Looking North)**

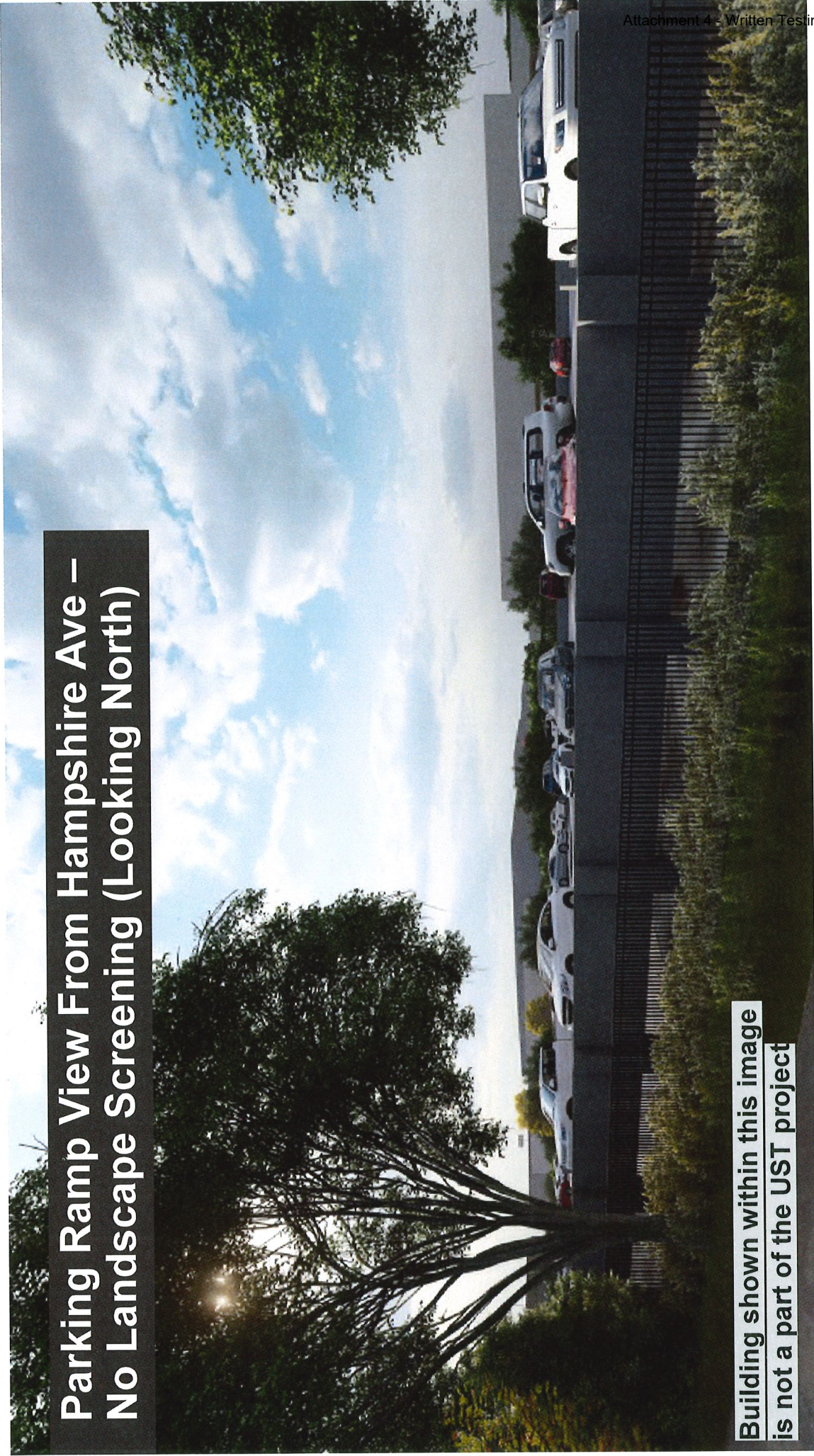


**Building shown within this image
is not a part of the UST project**

**Parking Ramp View From Cleveland
Avenue Bridge (Looking West)**

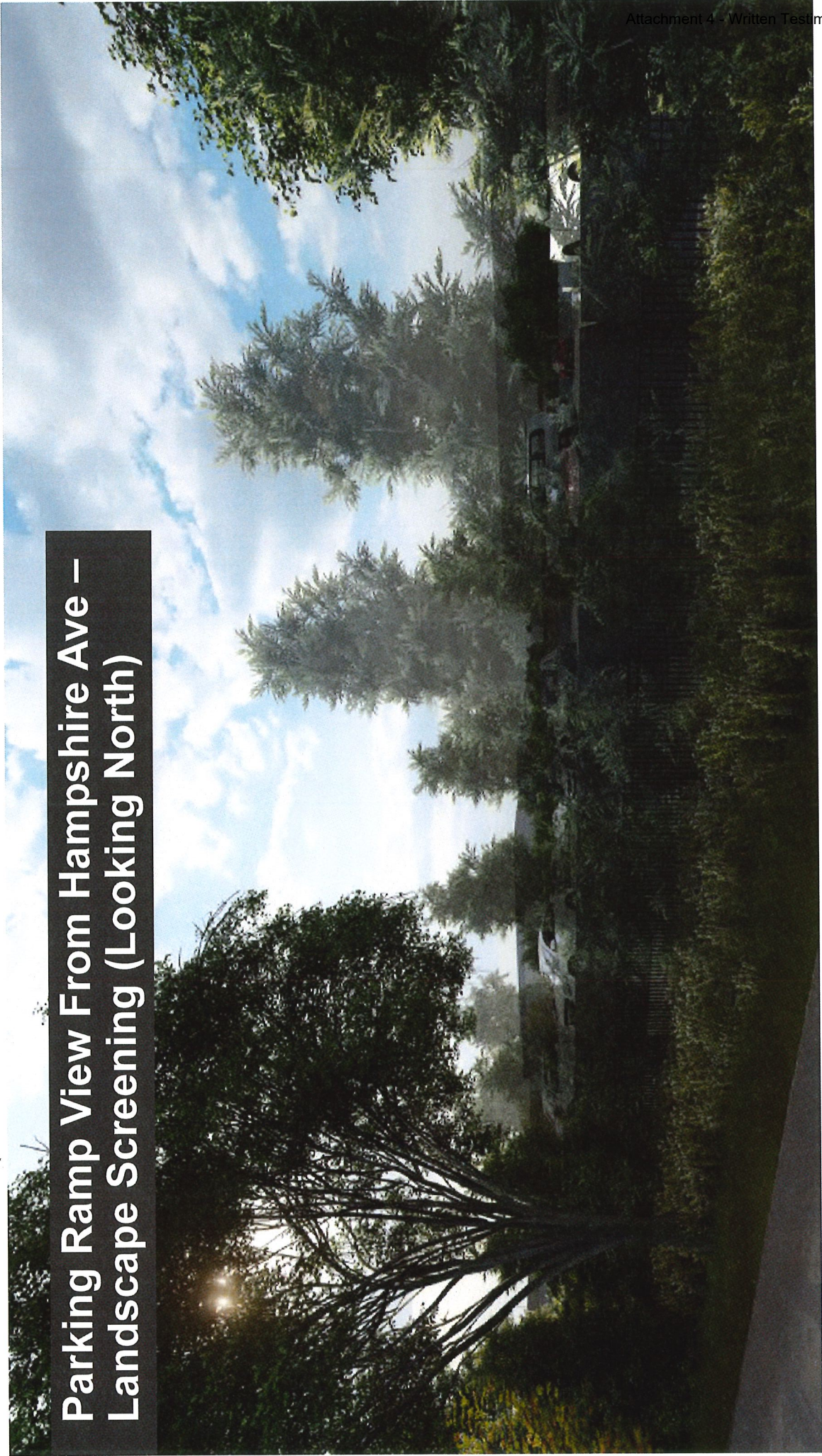


**Parking Ramp View From Hampshire Ave --
No Landscape Screening (Looking North)**



**Building shown within this image
is not a part of the UST project**

**Parking Ramp View From Hampshire Ave --
Landscape Screening (Looking North)**





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HIGHLAND
Bridge

Master Plan Amendment #4

Building Type Standards

HIGHLAND *Bridge* Master Plan Amendments

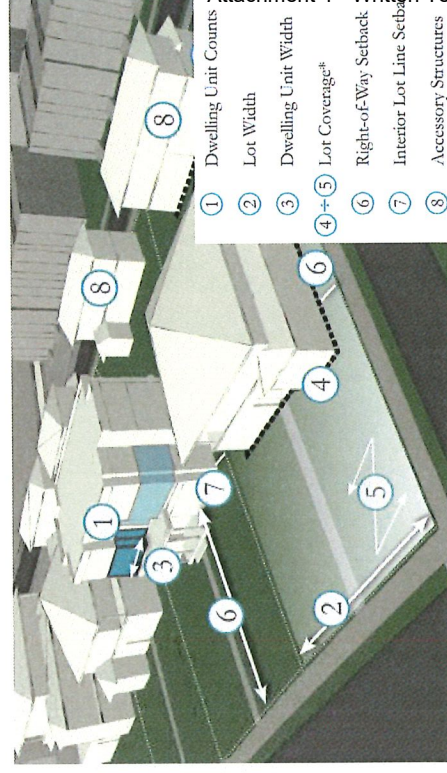
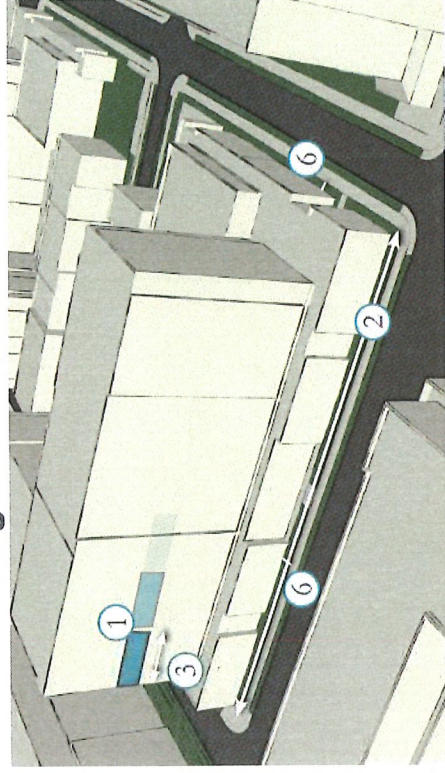


Master Plan Requirement: The Master Plan requires a maximum public right of way setback of 15 feet and that the maximum setback shall apply to at least 60% of the building façade along the right of way.

Requested Amendments: Exclusion to the maximum Public Right of Way Setback within the F6 district for Civic & Institutional uses.

Staff Recommendation: Exempt Civic & Institutional uses/buildings from the maximum ROW setback and building façade placement requirement in all F districts.

Images from Master Plan



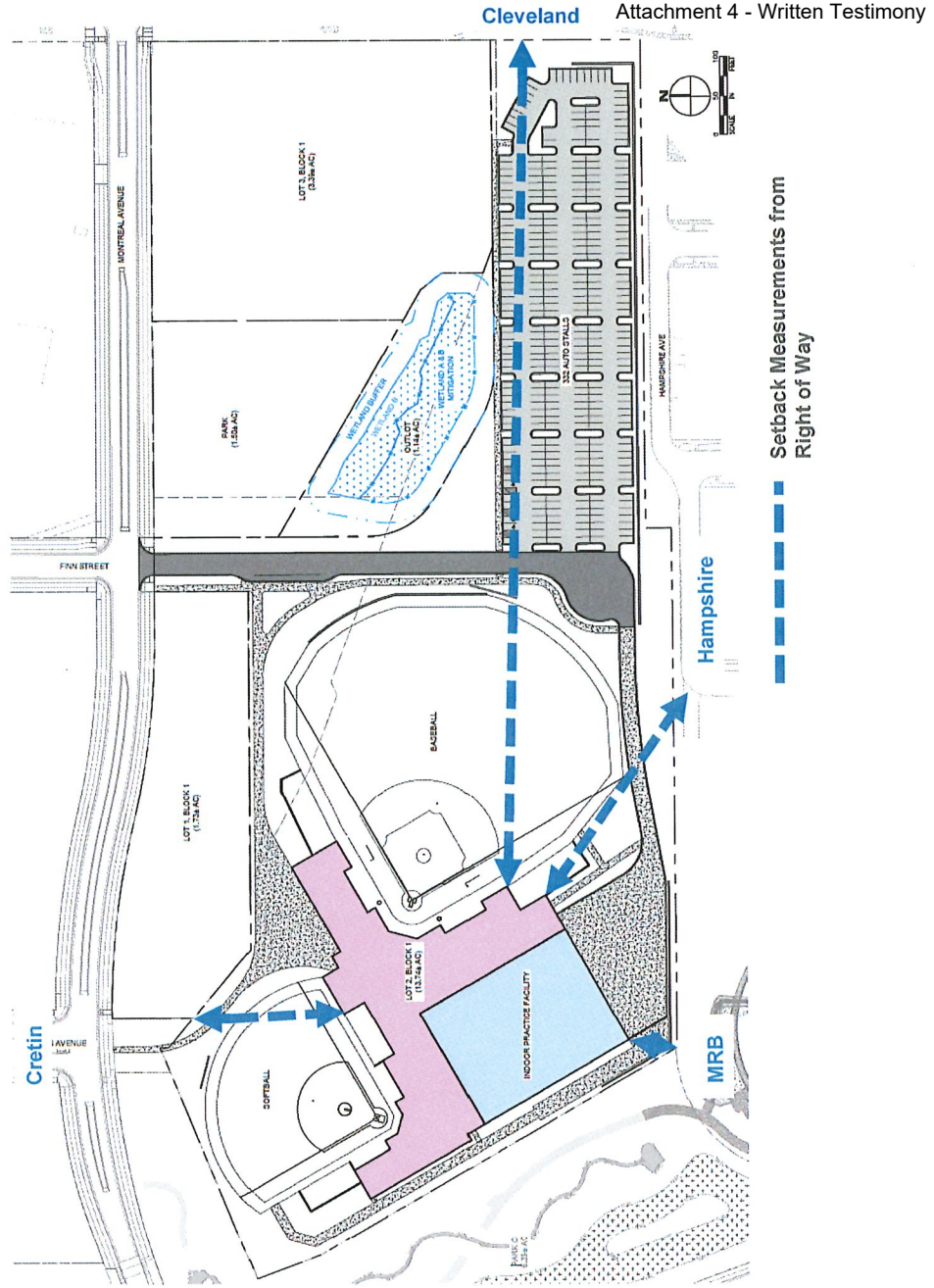
HIGHLAND *Bridge* Master Plan Amendments



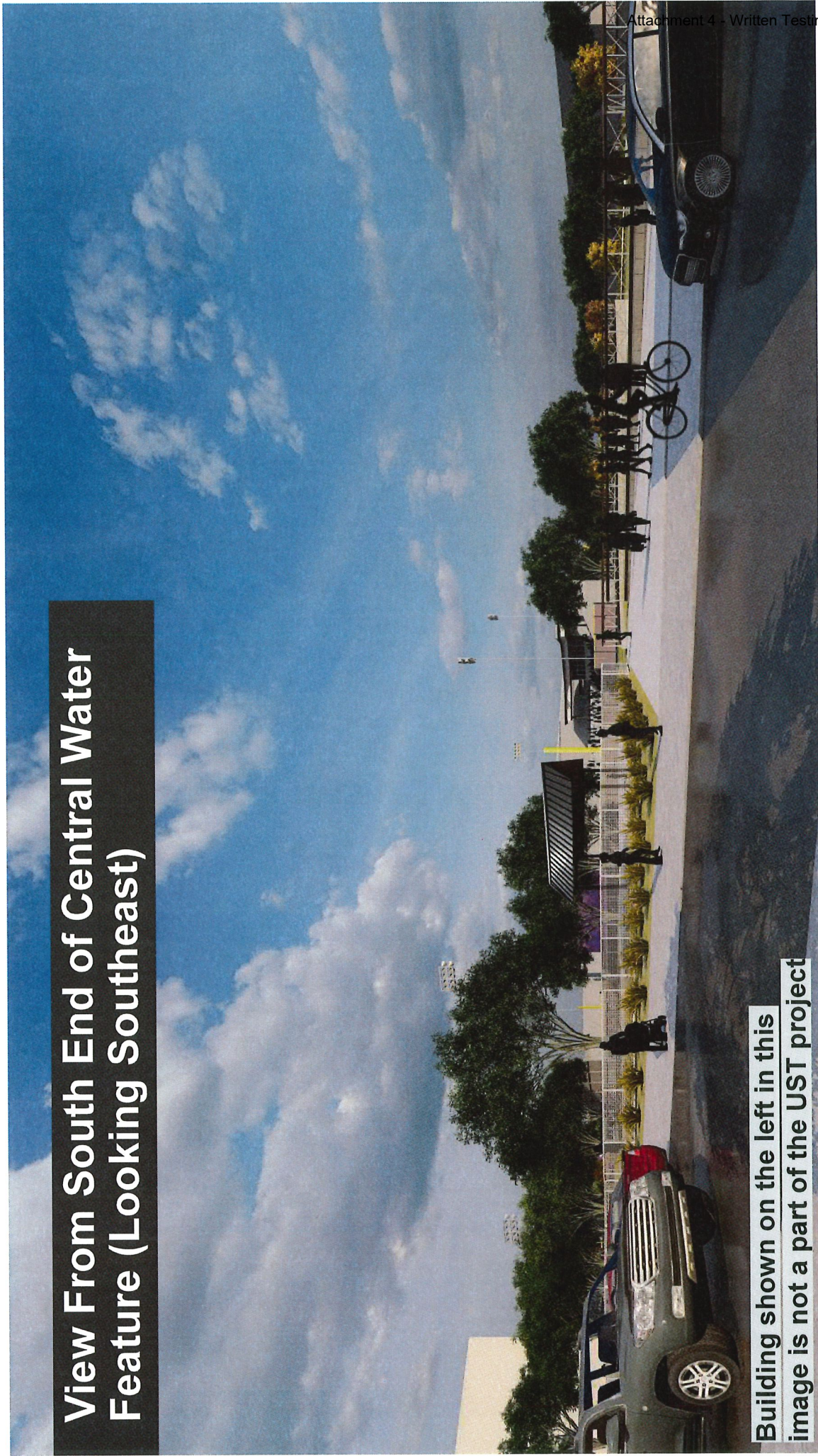
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Supporting Information:

- Civic & Institutional facilities are often set back from the public right of way to allow for preferred orientations of buildings, entry plazas and separation from vehicular traffic.
- Setbacks do not apply to the field areas of recreational facilities but rather the building elements of the facilities.

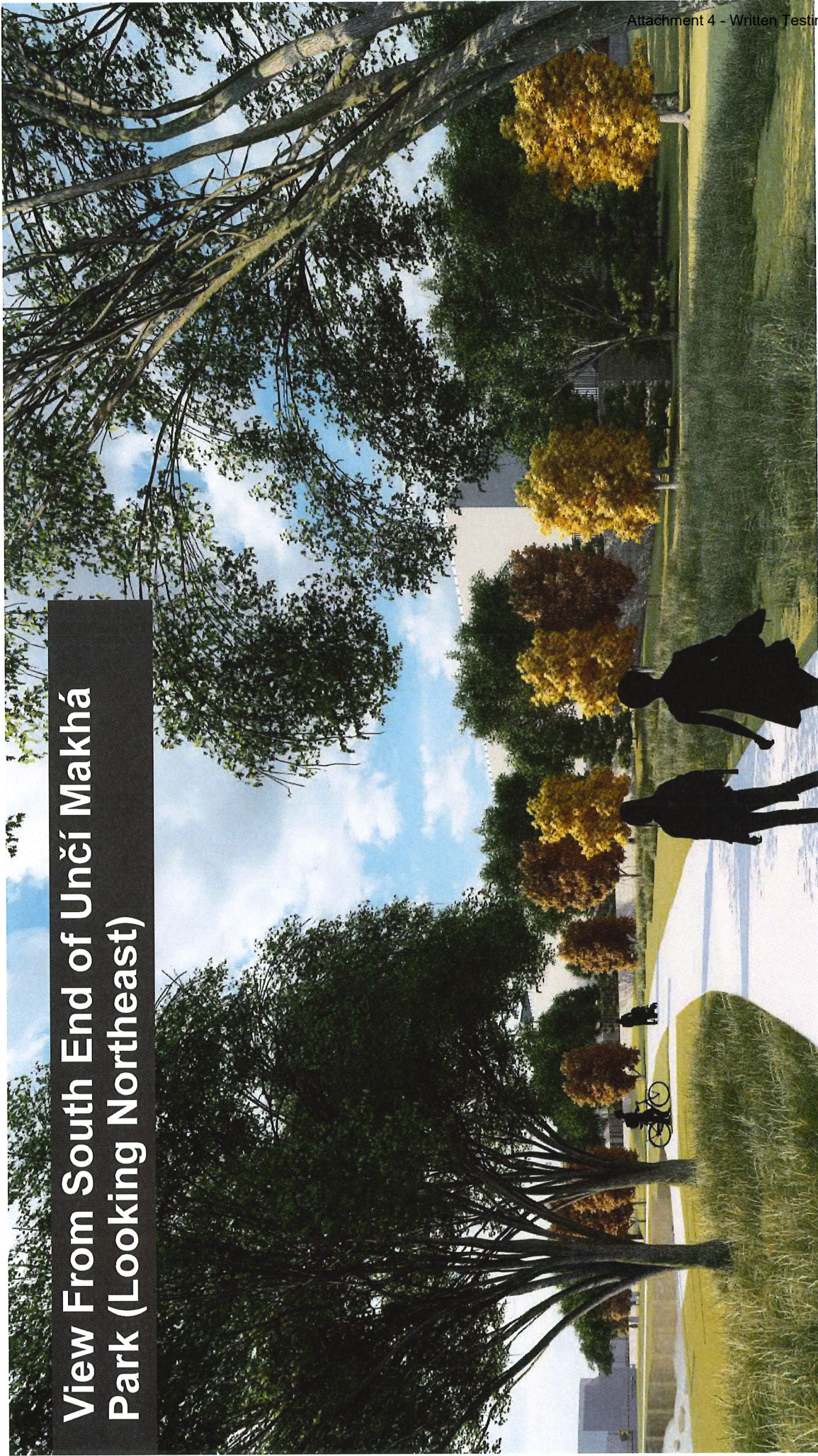


**View From South End of Central Water
Feature (Looking Southeast)**

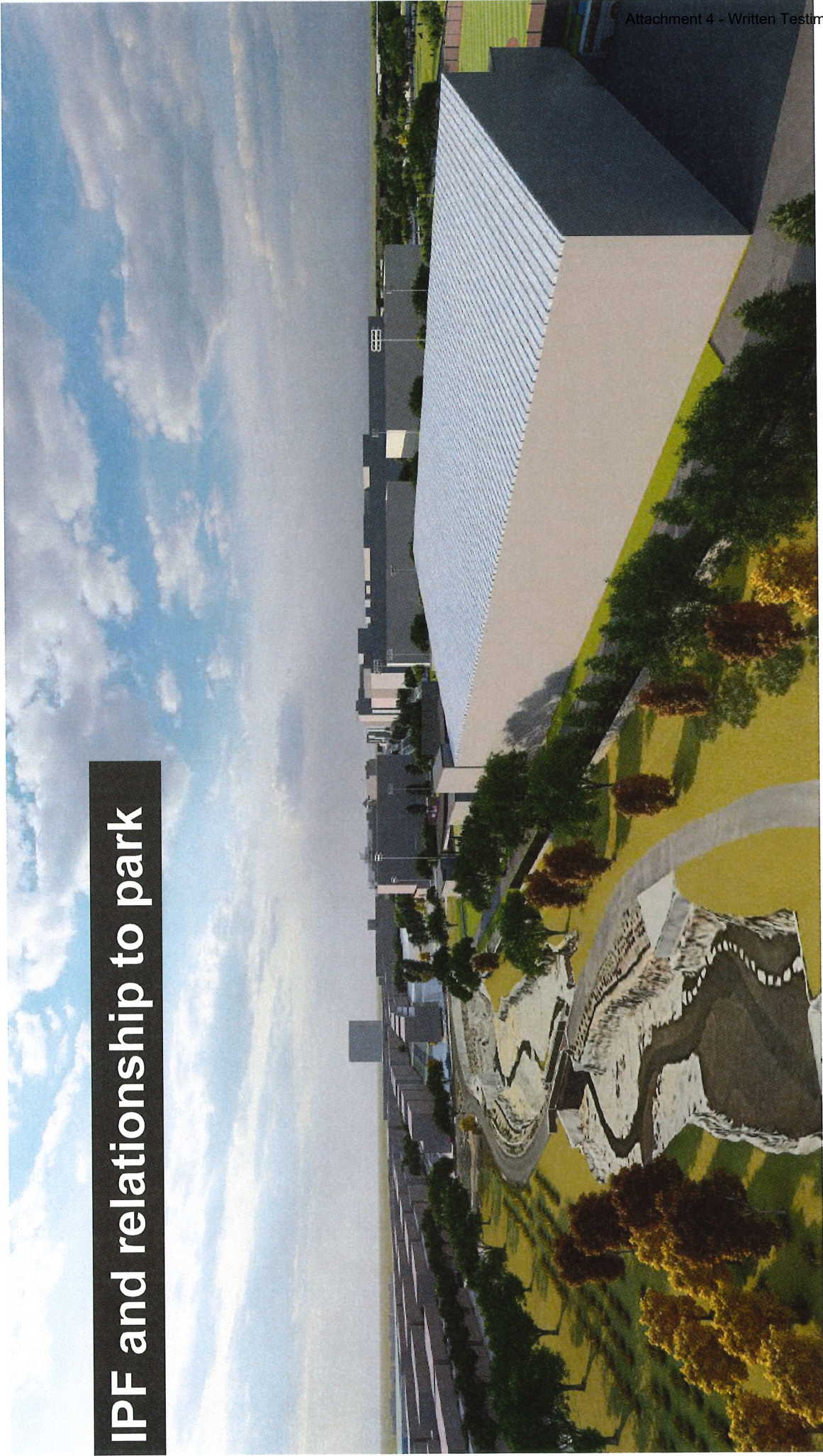


**Building shown on the left in this
image is not a part of the UST project**

**View From South End of Uncí Makhá
Park (Looking Northeast)**



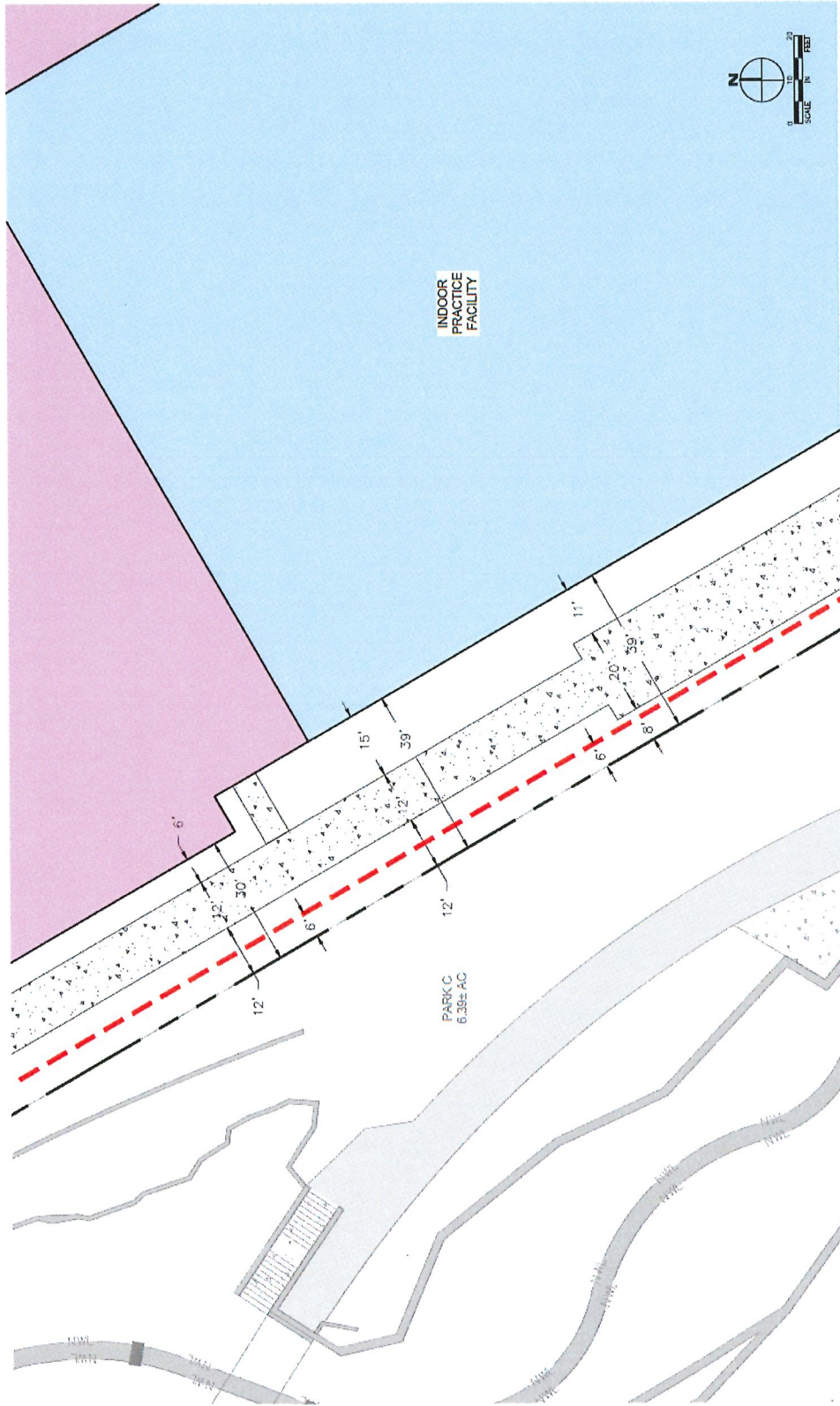
IPF and relationship to park





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Master Plan Amendment #5

Shared Transportation Corridor

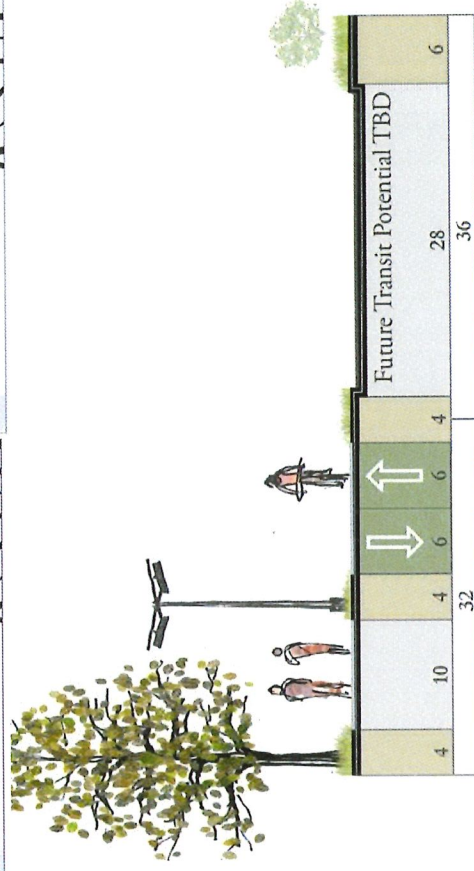
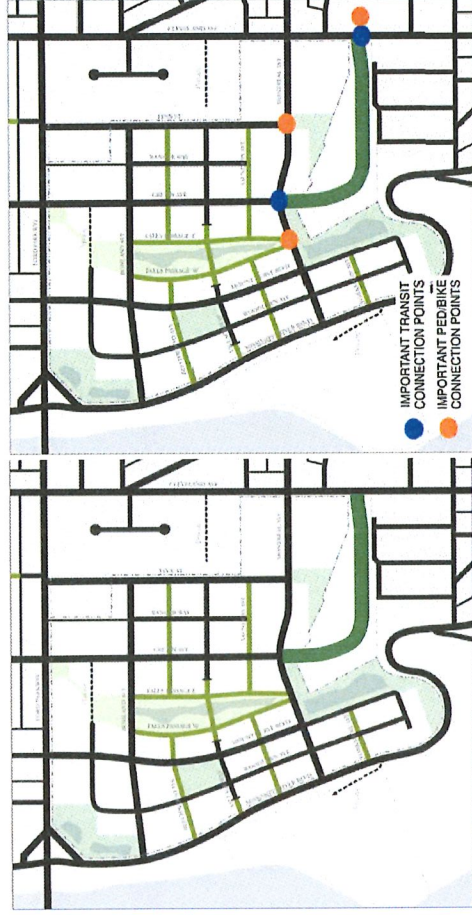
HIGHLAND Bridge Master Plan Amendments



Master Plan Requirement: The Master Plan requires a shared transportation corridor to connect Highland Bridge to the existing CP Rail corridor east of Cleveland Ave to provide corridors for walking, biking and transit.

Requested Amendments: Adjustment to allow for flexibility of alignment and information within the Shared Transportation Corridor description.

Staff Recommendation: Generalize the Shared Transportation Corridor narrative to highlight connection locations and modes. Clarify that development in this area must preserve pedestrian and bicycle connections from the CP Spur to Highland Bridge through the development site with opportunities for future transit service. Clarify that the location of public easement(s) must be agreed upon prior to final site plan approval. Remove the dimension table and unnecessary components of exhibits.



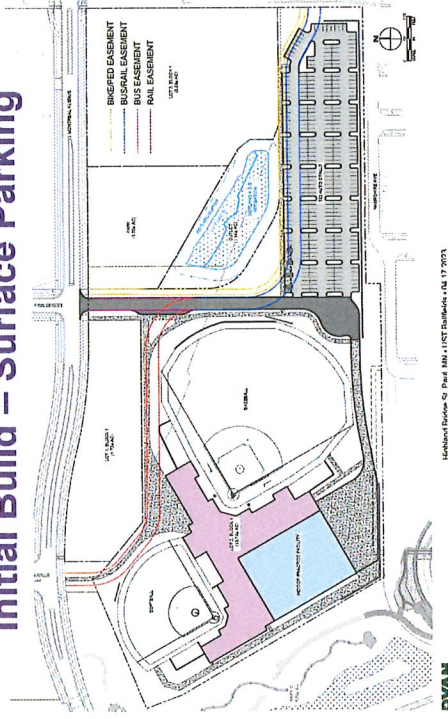
HIGHLAND *Bridge* Master Plan Amendments



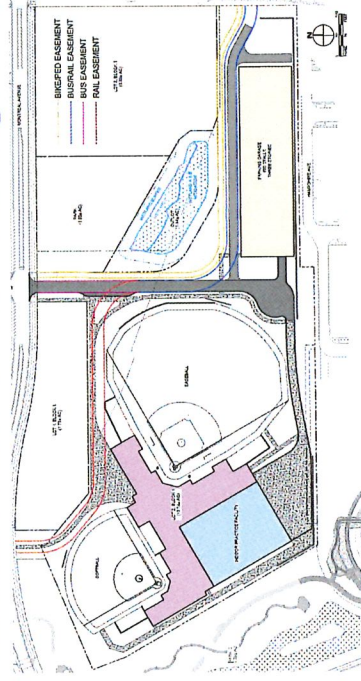
Supporting Information:

- The UST site plan allows for bike and pedestrian connections to extend north through Finn St or east/west through Montreal Ave.
- If bus is the form of transit used in the future, Cretin Ave and Montreal Ave are designed for this mode. Rerouting certain bus lines was included in the infrastructure design to connect Ford Pkwy to Cleveland Ave through the Highland Bridge site via Cretin and Montreal Ave.
- If shared rail is the form of transit, the UST site plan connection allows for sufficient turning radius within the site. Reconstruction of roadways and utilities would be needed to accommodate rail in the future.

Initial Build – Surface Parking



Potential Future Build – Parking Ramp



**Surface Parking View From Montreal Ave/Finn St.
Intersection (Looking South)**



**Parking Ramp View From Montreal Ave/Finn St.
Intersection (Looking South)**





Master Plan Amendment #6

Open Space System

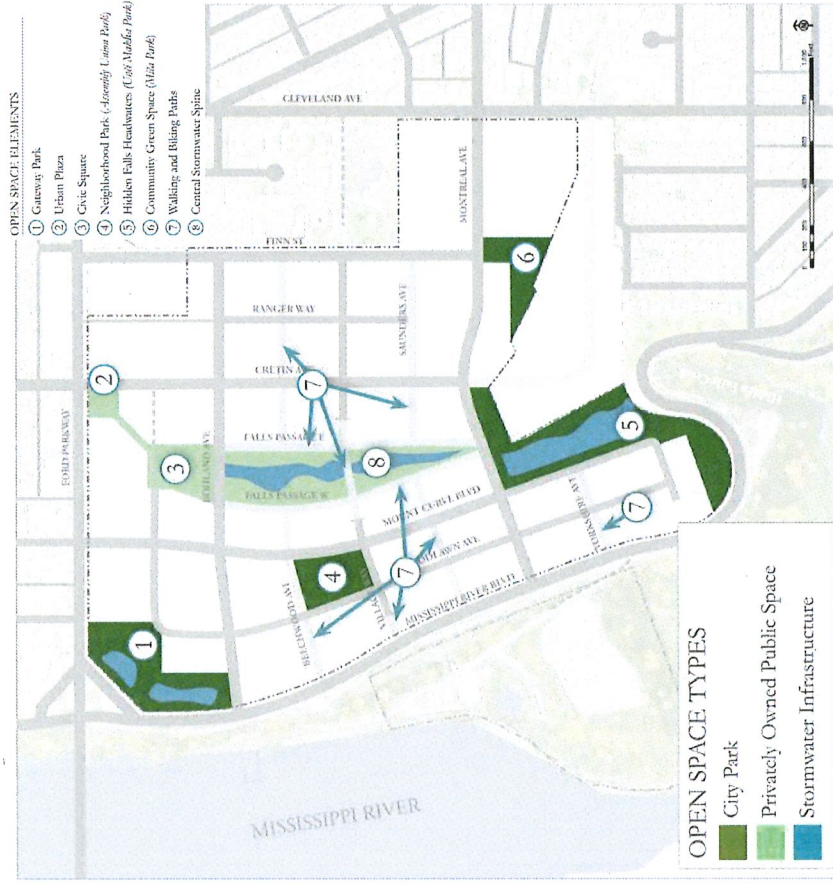
HIGHLAND *Bridge* Master Plan Amendments



Master Plan Requirement: The Master Plan includes locations and descriptions of various public open spaces within the development including city parks, privately owned public spaces and stormwater infrastructure

Requested Amendments: Adjustment to Open Space System for the proposed relocation of the Community Green Space and CP Rail Parkland Dedication.

Staff Recommendation: Relocate Mica Park to meet the needs of the proposed UST development and add open space to meet parkland dedication requirements, depicted in the Open Space System map. Update the Open Space System map and add language to clarify that final location and design are subject to approval by the City. Update the summary of Community Green Space (Mica Park) in the Master Plan. Defer to the Department of Parks and Recreation for final location and design.



HIGHLAND *Bridge* Master Plan Amendments



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Supporting Information:

- The new location of the Community Green Space is proposed east of the current location, southeast of the Montreal Ave and Finn St intersection.
- The new location allows the park to be moved to a more usable, functional space.
- The new parkland dedication will add to the park system already provided within Highland Bridge.



Park location shown subject to separate review and approval process by City of Saint Paul Parks.

Rendered Site Plan

Greyed out buildings to the north and east of the ballfields are not a part of the UST project and are only massed placeholders that comport with original redevelopment plan



Greyed out buildings to the north and east of the ballfields are not a part of the UST project and are only massed placeholders that comport with original redevelopment plan

Rendered Site Plan





HIGHLAND

DISTRICT COUNCIL

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HDC Resolution Master Plan Amendments at Highland Bridge

WHEREAS Ryan Companies has submitted an application to amend the Master Plan for the Highland Bridge site in order for the University of St. Thomas (UST) to develop a sports complex on the southern portion of the site; and

Whereas the Community Development Committee of the Highland District Council (HDC) has met with Ryan Companies, UST and neighbors on May 17, 2022, June 21, 2022, July 19, 2022, Oct 18, 2022, Jan. 24, 2023, Feb. 21, 2023 and March 21, 2023 to discuss the proposed plans for a UST sports complex on a portion of the Highland Bridge site, and 13 acres of the Canadian Pacific (CP) Rail land; and

Whereas the proposed sports complex is a use that was originally intended for the southern portion of the Highland Bridge site (2017 Master Plan design), but was changed when there was not a partner to develop the space; and

Whereas the City and Ryan have identified the need to amend the language of the Master Plan for this use to be built; and

Whereas six amendments to the Highland Bridge Master Plan are proposed:

- Exclusion to the Floor Area Ratio requirements within the F6 district for Civic & Institutional uses
- Include civic & institutional uses as a separate land use category with maximum parking per Sec. 63.207 of the Zoning Code
- Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasterly F6 district
- Exempt civic & institutional buildings from the ROW setback in the F6 district
- Revise the details of the Shared Transportation Corridor to allow flexibility for routing and engineering decision as design progresses
- Relocate Park through parkland diversion slightly to the east of th current location.
Address parkland dedication benefits package due to the development of the CP parcels.

Whereas the construction of ball fields will not meet the FAR because they are not as dense as an office building or other use, and

Whereas parking in the original plan was based on more of an urban use and limits the parking on the site to one space per 200 square feet of building, and this calculation does not work for a sports arena, which is based on seat count; and



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Whereas the construction of surface parking will benefit other groups, such as Highland Park Little League, and neighbors that use the other facilities; the parks, dog park, walking trails; and

Whereas the immediate neighbors to the south of the proposed plan have requested at numerous meetings that the proposed parking be a surface lot that does not impact their homes with light pollution, that the parking be recessed, and that sufficient foliage be added to minimize the impact; and

Whereas the Master Plan currently requires no more than a 15 foot setback, which applies to a more urban development, but with ball fields would not allow for proper operation, an entry plaza, and a proper setback from vehicle and pedestrian traffic; and

Whereas it is important to the community to maintain the Shared Transportation Corridor for future pedestrian/bike/transit uses, the committee encourages allowing for flexibility in the location of the corridor when planning for transit nodes, accessibility to bike trails and walking paths; and

Whereas the relocation of Parkland through a parkland diversion and the dedication of parkland benefits due to the development of the CP rail property, will allow for more usable space in Mica Park by taking away the challenge of the grade difference and wetland area of the currently proposed park; and

Whereas the neighbors and committee believe this project is making good use of land that has limited uses, would allow for further acquisition and use of the CP Rail spur, and with the airport overlay and bad grading, this is one of the best uses of this land; therefore

BE IT RESOLVED that the Community Development Committee of the Highland District Council recommends the approval of the following changes to the Highland Bridge Master Plan:

- Exclusion to the Floor Area Ration requirements within the F6 district for Civic & Institutional uses
- Include civic & institutional uses as a separate land use category with maximum parking per Sec. 63.207 of the Zoning Code
- Exempt civic & institutional uses from the 20 surface parking space maximum in the southeasterly F6 district
- Exempt civic & institutional buildings from the ROW setback in the F6 district
- Revise the details of the Shared Transportation Corridor to allow flexibility for routing and engineering decision as design progresses
- Relocate Park through parkland diversion slightly to the east of the current location. Address parkland dedication benefits package due to the development of the CP parcels.



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Approved March 21, 2023

By the Community Development Committee of the Highland District Council

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 |  1/3  | 

From: Tony Yazbeck <tyazbeck@c-dh.org>
Sent on: Tuesday, April 4, 2023 3:51:58 PM
To: Spencer.Miller-Johnson@ci.stpaul.mn.us
Subject: Baseball/Softball Facilities Letter of Support

Think Before You Click: This email originated **outside** our organization.

Tony Yazbeck
550 South Albert Street
Saint Paul, MN 55116
April 4, 2023

Planning Commission
City of St. Paul
25 4th St E
St. Paul, MN 55101

RE: University of St Thomas Ballfields Proposal, Zoning File No. 23-011-392 & 23-011-403

Dear St. Paul Planning Commission,

We are writing to express our support for the masterplan amendments for the University of St. Thomas Ballfields. Our Cretin-Derham Hall **community** believes that the addition of these ballfields will be a great asset and provide much-needed, high quality facilities for our community and youth sports in Saint Paul.

We appreciate the effort that has gone into the planning of these amendments and believe that they will be a positive addition to Highland Park and Saint Paul. We therefore encourage you to approve the proposed Master Plan amendments to allow for the University of St. Thomas Ballfields at Highland Bridge.

Thank you for your consideration.

Sincerely,

Tony Yazbeck

Activities Director

Cretin-Derham Hall

--



Tony Yazbeck, Assistant Athletic Director and Activities Director

tyazbeck@c-dh.org • 651-696-3342

[Cretin-Derham Hall Athletic](#) • [Cretin-Derham Hall](#) • 550 South Albert Street St. Paul, MN 55116 • 651-690-2443

CULTURE CREATES CHAMPIONS



[2023 Summer Sports Camps Registration Open](#)

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Tony Yazbeck
550 South Albert Street
Saint Paul, MN 55116
April 4, 2023

Planning Commission
City of St. Paul
25 4th St E
St. Paul, MN 55101

RE: University of St Thomas Ballfields Proposal, Zoning File No. 23-011-392 & 23-011-403

Dear St. Paul Planning Commission,

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We appreciate the effort that has gone into the planning of these amendments and believe that they will be a positive addition to Highland Park and Saint Paul. We therefore encourage you to approve the proposed Master Plan amendments to allow for the University of St. Thomas Ballfields at Highland Bridge.

Thank you for your consideration.

Sincerely,

Tony Yazbeck

Activities Director

Cretin-Derham Hall

I'd like to introduce myself briefly before I get to the issue at hand - my name is Rannon Arch, I live at 1026 Bowdoin St in Highland Park with my wife. We recently bought our property in early October of 2022 and love the quaint, small town feel of our neighborhood. In fact, we specifically purchased in this area due those endearing characteristics as it is hard to find in the Twin Cities. Our property is just two houses down from "the berm" that separates the residential homes and the proposed site of the University of St. Thomas (UST) and Ryan Companies venture to build a baseball/softball complex.

Our neighborhood has been meeting as a group and has even engaged in many conversations with UST and Ryan Companies about this project. I have personally been very impressed with how they are managing the situation with us to keep the feel of our neighborhood intact despite the anticipated construction and proposed changes they seek to make. We recently learned that one of the amendments UST/Ryan Companies has proposed to the Ford site master plan is to allow for a larger surface parking lot that will be able to hold more than 20 cars. In response to this proposed amendment we understand that the Saint Paul Planning Commission is not in favor of this proposed amendment and is instead asking UST to have plans to build a parking ramp.

As someone who lives quite close to this new parking area, I hope what I have to say next carries weight with you, but I vehemently oppose your desire to ask UST/Ryan Companies to have to build a parking ramp/garage in this project. This parking garage will be an obvious eyesore in our neighborhood and is not inline with the setting of an older, quiet residential neighborhood. It is in direct opposition to what we as neighbors have discussed and makes us all quite nervous/scared that this is even being considered.

I urge you to allow the passage of the proposed amendment that UST/Ryan Companies has asked for in building a larger surface parking lot area. This surface parking will have a lower impact to our neighborhood (i.e. noise/light pollution, congestion, etc), it will still allow for larger civic and institutional uses, and it will maintain the feel of our community/neighborhood. Please please take into perspective what you are asking for here. Many folks in this area have been long-time home owners and are not pleased (put mildly) with the proposed idea of a large parking garage in our neighborhood.

If you have any questions about my stance, please feel free to connect as I am happy to further discuss my thoughts around this issue. Look forward to Friday's public hearing.

Rannon Arch

1026 Bowdoin St

Writing to let you know that along with 100% of my neighbors that live adjacent to this project, we are not in favor of a parking ramp and would push the idea of more surface lot space. Enough St. Paul neighborhoods have been impacted and degraded because of big money construction. Thank you.

Rob Gruber

1044 Bowdoin St

Dear St. Paul Planning Commission,

I am writing to comment on the proposed amendment to the Highland Bridge Master Plan that would allow civic and institutional entities to have surface parking lots larger than the 20-car maximum currently stipulated in the Master Plan.

I endorse the amendment, which then would allow the University of St. Thomas to build a 330-car surface lot as part of its softball and baseball complex in the southeast corner of Highland Bridge.

I have lived at 1032 Bowdoin Street for nearly three decades. My home is three houses south of the border that separates Highland Bridge and our neighborhood. I have attended dozens of meetings in recent years regarding Highland Bridge and formerly served on the Highland District Council.

I can understand why the 20-car limit for surface lots was stipulated in the original master plan, but that was before anyone envisioned something like a softball and baseball complex in the former train yard of the Ford plant. This corner of the Highland Bridge project is distinct from the original Highland Bridge development, and in this case ample parking is clearly needed for the baseball project to work.

If St. Thomas was forced to build a ramp, it would be right at the end of my street and would completely change the character and feel of our neighborhood. I am active in my neighborhood, and I have not talked to a single neighbor who wants to see a parking ramp at the end of Bowdoin Street.

The parking lot proposed by St. Thomas would have many community benefits. It would be used by those attending the little league baseball fields just to the north of Montreal, and in turn would reduce or eliminate the parking and congestion in the neighborhood that we now experience when the little league games are held. It would be used by those visiting the new park and wetlands that will be located immediately north of the surface lot. It also could be used by those who use the new dog park and who visit the Highland Bridge water features.

Most important to those who live in the neighborhood immediately south of Highland Bridge, free and ample parking at the baseball complex will help ensure that people don't use our neighborhood as their parking lot. Preventing St. Thomas to have a surface lot would, in effect, kick the parking problem out of Highland Bridge and into surrounding neighborhoods.

St. Thomas has an undisputed reputation for building quality facilities, and Ryan Companies has shown it can design and build infrastructure the city will be proud of for decades to come. I have no doubt that the St. Thomas-Ryan team will come up with an attractive and well-used surface lot.

I live at 1032 Bowdoin Street just three houses away from the proposed location of the University of St. Thomas baseball and softball facilities. Our street basically dead ends into the location of a proposed surface parking lot that would provide parking for the St. Thomas facilities as well as the new proposed park and the existing Ford ball fields.

I am writing to ask the St. Paul Planning Commission to support the amendment that would allow St. Thomas to build this surface parking lot.

The planned surface lot would be much less intrusive to the surrounding area, especially our street, as it will be located about 15 feet below the sightline of our neighborhood. I am one of 20 homeowners who live near this location who support the facility that St. Thomas is proposing and we are all in agreement that the surface parking lot for 300+ cars is the best option.

We are against building a parking ramp on the site as it will be visible to our neighborhood and will contribute to more noise and light pollution than the proposed surface lot. The surface lot will be lower, have trees and vegetation and will generally be a better fit for our neighborhood.

Building the parking lot is 100% necessary to keep on-street parking to a minimum in the surrounding neighborhoods. We already deal with this when there are games at the Ford ball fields and those events are much smaller than anything that would be held at the St. Thomas fields. Cars along Cleveland Ave. and Montreal Ave. during little league games, children running into the street, etc. make it fairly chaotic. We also don't want to perpetuate having spectators of the St. Thomas games parking in our neighborhood along Hampshire, Bowdoin, Finn and Colby simply because there is no other option for them.

This 300+ parking lot that can be used by the Ford little league, park goers and St. Thomas is the best option for all of us who call this area of St. Paul home and for the people who will visit these facilities in the future.

Please vote to pass this amendment.

Thank you,

Lisa Erbes

1032 Bowdoin St.

St. Paul 55116

678-772-8945

lerbes@gmail.com

From: pam ginther <plmginth@comcast.net>
Sent: Thursday, April 27, 2023 10:12 PM
To: Spencer Miller-Johnson <Spencer.Miller-Johnson@ci.stpaul.mn.us>
Subject: Request: Prefer a surface parking lot rather than a ramp.

Attachment 4 - Written Testimony

Think Before You Click: This email originated outside our organization.

Dear Spencer Miller-Johnson City of St. Paul:

For over 30 years my family and I have lived at
1019 Colby St.

1

St. Paul, MN 55116

We are directly next to the CP Rail property.

My family and I are writing to ask you to support UST in building a 300 space surface parking lot for their new baseball and softball complex, rather than a parking ramp. The surface lot not only fulfills their parking needs, but those of Highland Little League, and the general public visiting Mica park. It will also alleviate parking congestion that would happen for neighbors that live along Cleveland and the corners of the new Montreal area.

Moreover it will help replace some of the existing environment, trees and woodlands that have been, and will be further impacted by more construction in our collective neighborhoods.

Building a surface lot will allow UST and Ryan to mitigate the loss of the original trees and woodlands by landscaping with new trees for cooling, shade, and wind shelter, while maintaining some habitat for nature and visual beauty.

This is more vital that ever to the health of our environment, and the health of human and animal families.

It will create a soft visual edge from all sides. Just like the parking areas that serve Minnehaha and Nokomis Parks. It will blend the practicality for parking while respecting the the needs of beauty and nature. Which is what St. Paul and Mpls have been known for across the country.

We are asking you to note the collective wins mentioned above and add one more vital win for the environment. Please blend old and new with the soft edges of nature. Allow for the creation of a parking lot that serves all of us with the practicality of parking, while respecting the the needs of our health and environment.

Thank you for your consideration,
The James Ginther Family

From: jcwinterer@gmail.com <jcwinterer@gmail.com>
Sent: Tuesday, April 25, 2023 10:19 AM
To: Spencer Miller-Johnson <Spencer.Miller-Johnson@ci.stpaul.mn.us>
Subject: Comment on admendment to Highland Bridge master plan

Attachment 4 - Written Testimony

Think Before You Click: This email originated **outside** our organization.

April 25, 2023

Dear St. Paul Planning Commission,

I am writing to comment on the proposed amendment to the Highland Bridge Master Plan that would allow civic and institutional entities to have surface parking lots larger than the 20-car maximum currently stipulated in the Master Plan.

I endorse the amendment, which then would allow the University of St. Thomas to build a 330-car surface lot as part of its softball and baseball complex in the southeast corner of Highland Bridge.

I have lived at 1032 Bowdoin Street for nearly three decades. My home is three houses south of the border that separates Highland Bridge and our neighborhood. I have attended dozens of meetings in recent years regarding Highland Bridge and formerly served on the Highland District Council.

I can understand why the 20-car limit for surface lots was stipulated in the original master plan, but that was before anyone envisioned something like a softball and baseball complex in

the former train yard of the Ford plant. This corner of the Highland Bridge project is distinct from the original Highland Bridge development, and in this case ample parking is clearly needed for the baseball project to work.

If St. Thomas was forced to build a ramp, it would be right at the end of my street and would completely change the character and feel of our neighborhood. I am active in my neighborhood, and I have not talked to a single neighbor who wants to see a parking ramp at the end of Bowdoin Street.

The parking lot proposed by St. Thomas would have many community benefits. It would be used by those attending the little league baseball fields just to the north of Montreal, and in turn would reduce or eliminate the parking and congestion in the neighborhood that we now experience when the little league games are held. It would be used by those visiting the new park and wetlands that will be located immediately north of the surface lot. It also could be used by those who use the new dog park and who visit the Highland Bridge water features.

Most important to those who live in the neighborhood immediately south of Highland Bridge, free and ample parking at the baseball complex will help ensure that people don't use our neighborhood as their parking lot. Preventing St. Thomas to have a surface lot would, in effect, kick the parking problem out of Highland Bridge and into surrounding neighborhoods.

St. Thomas has an undisputed reputation for building quality facilities, and Ryan Companies has shown it can design and build infrastructure the city will be proud of for decades to come. I have no doubt that the St. Thomas-Ryan team will come up with an attractive and well-used surface lot.

For all these reasons, please pass the amendment and allow the university to build the surface lot.

Sincerely,

Jim Winterer
1032 Bowdoin St.
St. Paul, MN 55116
jcwinterer@gmail.com

From: Jeanette Bloss <jeanette.bloss@gmail.com>

Sent: Thursday, April 27, 2023 10:15 PM

To: Spencer Miller-Johnson <Spencer.Miller-Johnson@ci.stpaul.mn.us>

Subject: Ford Development - Surface Parking Lot for St. Thomas Ballfields

Attachment 4 - Written Testimony

Think Before You Click: This email originated **outside** our organization.

Greetings Mr. Miller-Johnson,

I own and live in a home directly south of the Ford Redevelopment area. I'm reaching out in support of the master plan amendment for the Highland Bridge development that would allow St Thomas to build a surface parking lot instead of a multilevel parking garage as the parking area for their proposed ballfields.

First and foremost for me is that the surface lot would require the planting of trees in islands and areas surrounding the lot while a parking garage would not. It's not just about the look of greenery versus no greenery, those trees would actually mean something and do something, which is to provide shade and reflect back less sun and heat into the air, in turn creating a slight cooling effect. A parking garage would not have that benefit; density (such as a parking garage) without trees has a detrimental effect on neighborhood temperatures.

Second is the usefulness/need of a surface lot versus a parking garage. The number of users of the lot is unlikely to exceed the proposed surface lot plan - there isn't need for a larger, multi-level parking garage. It would take more time and resources to build which would likely delay the construction until the ball fields themselves were closer to readiness, which could take years. On the other hand, a surface lot is easier to put in with less resources, time, and expense required so it could be built before the ballfields are ready and support the Highland Ball games, dog park, and in-development park and wetlands sooner. If usage got to be more than the surface lot could support, then a parking garage could go in later. But if the parking area starts as a garage and then doesn't get enough usage, then not only

would it be a lot harder to demolish the structure if needed, but it would also be unnecessary sprawl: building for the sake of building with, again, much fewer trees.

Finally, I expect a surface lot to be more secure or at least less attractive as a destination for car break ins or other crime. Whether a parking garage or a surface lot is put in, St. Thomas is only planning on staffing security for their events at the ballfields, about 50 days out of the year. The rest of the time, the parking area would be unsecured without security staff. As an enclosed space, a parking garage inherently has less visibility and without regular security I'd be pretty concerned that would make the garage fairly attractive for property crimes, more so than a surface lot anyway.

Thanks for taking the time to read this and review my concerns. I appreciate your consideration and that of the planning commission with regard to this proposed amendment to the Highland Bridge development master plan.

Sincerely,
Jeanette Bloss



Virus-free www.avast.com

Karoline Finlay

From: Jeanette Bloss <jeanette.bloss@gmail.com>
Sent: Friday, April 28, 2023 7:38 AM
To: Karoline Finlay
Subject: Re: Ford Site/Highland Bridge Master Plan Amendments
Attachments: image001.png

Think Before You Click: This email originated outside our organization.

Greetings Ms. Finlay,

Thank you for your email. My address is:
1055 Finn St S
Saint Paul, MN 55116

Thank you,
Jeanette Bloss

On Fri, Apr 28, 2023, 7:32 AM Karoline Finlay <karoline.finlay@ci.stpaul.mn.us> wrote:

Ms. Bloss:

Thank you for your written testimony regarding the Ford Site/Highland Bridge Master Plan Amendments. Please respond to this email with your address. Without it we will not be able to include your testimony in the public record.

Thank you,

Karoline.

Karoline Finlay

Planning Secretary

Planning and Economic Development

25 4th St. W., 1400 City Hall Annex

Saint Paul, MN 55102

651-266-6641